



City of Riverbank  
**Planning Commission Meeting**  
City Hall North • Council Chambers  
6707 Third Street • Suite B • Riverbank • CA 95367

## Agenda

Tuesday, January 17, 2017 – 6:00 P.M.

<b><u>CALL TO ORDER:</u></b>	<b>Chair:</b>	<b>McKinney</b>
<b><u>ROLL CALL:</u></b>	<b>Chair:</b>	<b>McKinney</b>
	<b>Vice Chair:</b>	<b>King</b>
	<b>Commissioner:</b>	<b>Stewart</b>
	<b>Commissioner:</b>	<b>Tabacco</b>
	<b>Commissioner:</b>	<b>Villapudua</b>

### CONFLICT OF INTEREST

Any Planning Commission Member or Staff who has a direct Conflict of Interest on any scheduled agenda item to be considered is to declare their conflict at this time.

#### 1. PUBLIC COMMENTS (No action to be taken)

At this time, members of the public may comment on any item not appearing on the agenda, and within the subject matter jurisdiction of the Planning Commission Board. Individual comments will be limited to a **maximum of 5 minutes** per person and each person may speak once during this time; time cannot be yielded to another person. Under State Law, matters presented during the public comment period cannot be discussed or acted upon. For record purposes, state your name and City of residence. Please make your comments directly to the Planning Commission Board.

#### 2. CONSENT CALENDAR

All items listed on the Consent Calendar are to be acted upon by a single action of the Planning Commission Board unless otherwise requested by an individual Planning Commissioner Member for special consideration. Otherwise, the recommendation of staff will be accepted and acted upon by roll call vote.

**Item 2.A:** Posting of the Agenda. The Agenda for the January 17, 2017 Planning Commission Meeting was posted on the City Community Center bulletin board, City Hall North & South bulletin boards, Post Office, city website and emailed to the Library on January 12, 2017.

**Item 2.B:** Approval of the Agenda. This provides an opportunity for the Planning Commission or Staff to recommend that an item be placed on the agenda for discussion or to adjust the proposed agenda to allow an item to be taken out of order.

**Item 2.C:** Approval of the Minutes of the December 20, 2016 Planning Commission Meeting, having been read by the individual Commissioners and stands approved as submitted. *Abstain from voting: Chair McKinney.*

**Recommendation:** It is recommended that Planning Commission approve the Consent Calendar by roll call vote.

### 3. PUBLIC HEARINGS

*The Public Hearing Notice for Public Hearing Items 3.1 & 3.2 was published in the Riverbank News on 1/4/2017.*

**Item 3.1: CUP 02-2016 – Mobilitie, LLC** – Request to install antennas within city light poles within city rights-of-way at various locations throughout the city, including one (1) located at Oakdale Road and Patterson Road. Site ID: 9CAB009302-A. CEQA Categorical Exemption: Article 19, Section 15332 In-Fill Development Projects.

**Recommendation:** Approval by roll call vote

**Item 3.2: Architecture and Site Plan Review 03-2016 and Conditional Use Permit 02-2016 – Jason Huckabee** – Request for architecture and site plan review of the Del Rio Theater building located at 3300 Atchison within the Downtown Specific Plan area. The proposed project includes a remodeled restaurant/piano bar, coffee shop with drive-thru, and banquet hall, a Conditional Use Permit for serving alcohol and a future lot merge. APN: 132-009-001. CEQA Categorical Exemption: Article 19, Section 15332 In-Fill Development Projects.

**Recommendation:** Approval by roll call vote

**Continued Item 3.3: Revocation of Conditional Use Permit No. 05-2007 and Business License -** Action on Code Enforcement Case #2015-0209 for 209 Concepts – **Location:** 6607 Callander Avenue, APN: 075-004-046. Repeated violations of the City’s zoning code and conditions of approval of CUP 05-2007 have resulted in the City’s decision to move forward to revoke the conditional use permit and business license on the property. Business owner was given a 30 day extension by the Planning Commission at the December 20, 2016 meeting.

**Recommendation:** Approval by roll call vote

### 4. PLANNING COMMISSION

**None.**

### 5. COUNTY REFERRAL/CORRESPONDENCE/INFORMATION (Information Only – No Action)

**None.**

### 6. UP-COMING MEETING AGENDA ITEMS (Information Only – No Action)

**Item 6.1:** Development Agreement 01-2016 – Chris Hayes – approval of a Development Agreement between the City and the developer of the Hayes 4 Phase 1 subdivision. Property is located at Claus Road and Central Avenue. APN 062-022-022/-019.

**Item 6.2:** Architecture and Site Plan Review 02-2016 – Jason Huckabee – request for architecture and site plan review of a new 2,500 square foot office building located at 6800 Third Street (Reed’s Garage site) within the Downtown Specific Plan area. A second, similar building is proposed for future construction on the same site. APN: 132-009-023.

### 7. ADJOURNMENT – The next regular Planning Commission meeting – February 21, 2017 @ 6:00 p.m.

## AFFIDAVIT OF POSTING

*I, Janet Smullen, do hereby certify under penalty of perjury, under the laws of the State of California that the foregoing agenda was posted 72 hours to the meeting in accordance to the Brown Act.*

*Posted this 12<sup>th</sup> day of January, 2017  
Janet Smullen, Sr. CDS*

**NOTICE REGARDING AMERICANS WITH DISABILITIES ACT:** In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the Development Services Department at (209) 863-7128 or [jsmallen@riverbank.org](mailto:jsmallen@riverbank.org). Notification 72-hours before the meeting will enable the City to make reasonable arrangements to ensure any special needs are met. [28 CFR 35.102-35.104 ADA Title II].

**NOTICE REGARDING NON-ENGLISH SPEAKERS:** Pursuant to California Constitution Article III, Section IV, establishing English as the official language for the State of California, and in accordance with California Code of Civil Procedures Section 185, which requires proceedings before any State Court to be in English, notice is hereby given that all proceedings before the City of Riverbank City Planning Commission shall be in English and anyone wishing to address the Planning Commission is required to have a translator present who will take an oath to make an accurate translation from any language not English into the English language.

**GENERAL INFORMATION:** The Riverbank Planning Commission meets on the third Tuesday of each month at 6:00 p.m.

**COMMISSION AGENDAS:** The Planning Commission agenda is posted pursuant to the California Brown Act, which only requires these agenda title pages to be posted near the entrance of the location where the meeting is to be held and, when technologically able, on the City's website. Additional documents may be provided by the City in its efforts of transparency to keep the public well informed. The agenda packet (agenda plus supporting documents) are posted for public review at the Development Services Department, 6617 Third Street, Riverbank, CA and at [www.riverbank.org](http://www.riverbank.org) upon distribution to a majority of the Planning Commission. A subscription to receive the agenda can be purchased for a nominal fee through the City Clerk's Office.

**PUBLIC HEARINGS:** In general, a public hearing is an open consideration within a meeting of the Planning Commission Board, for which special notice has been given and may be required. During a specified portion of the hearing, any resident or concerned individual is invited to present protests or offer support for the subject under consideration.

**Televised/Video of Meetings:** Charter – Channel 2; AT&T Uverse – Channel 99  
Visit [www.riverbank.org](http://www.riverbank.org) to connect to meeting videos. (Note: Technical difficulty occurs on occasion preventing the televising or recording of the meeting.)

**City Hall Hours:** City Hall is open Monday – Thursday: 7:30 am – 5:30 pm and Fridays: 8:00 am – 5:00 pm; CLOSED alternating Fridays.

**QUESTIONS:** Contact the Developmental Services Department at (209) 863-7128.

Any documents that are not privileged or part of a Closed Session provided to a majority of the Planning Commission after distribution of the agenda packet, regarding any item on this agenda, will be made available for the public inspection at North City Hall, 6707 Third Street, Riverbank, CA during normal business hours.



## City of Riverbank

6707 Third Street • Riverbank, CA 95367-2396  
Phone: (209) 869-7128 • Fax: (209) 869-7126

# NOTICE OF CONTINUANCE

AFFIDAVIT OF POSTING NOTICE OF CONTINUANCE:

**Item 3.3: Revocation of Conditional Use Permit No. 05-2007 and Business License / Resolution 2016-020** - Action on Code Enforcement Case #2015-0209 for 209 Concepts – **Location:** 6607 Callander Avenue, APN: 075-004-046. Repeated violations of the City’s zoning code and conditions of approval of CUP 05-2007 have resulted in the City’s decision to move forward to revoke the conditional use permit on the property.

### **POLICY FOR PUBLIC NOTICE POSTING REQUIREMENTS**

I, JANET SMALLEN, SR. COMMUNITY DEVELOPMENT SPECIALIST, declare as follows:

That I am the Sr. Community Development Specialist of the City of Riverbank. That at a regular meeting of the City Planning Commission of the City of Riverbank held December 20, 2016, said Public Hearing Item was continued to the January 17, 2016 Planning Commission Meeting.

I declare under penalty of perjury that the foregoing is true and correct.

Dated this 22<sup>nd</sup> day of December 2016.

/s/ Janet Smallen,  
Sr. Community Development Specialist  
City of Riverbank

*For official use only:*

Posted: 12/22/16	Publish: n/a	Remove: 01/18/17
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## City of Riverbank Development Services Department

Planning Division ≈ Building Division ≈ Neighborhood Improvement Division

6707 Third Street, Riverbank, CA 95367 Office (209) 863-7128 FAX (209) 869-7126

**PUBLISH DATE:** January 4, 2017  
**DEPT:** PLANNING

**LEGAL**

### **CITY OF RIVERBANK NOTICE OF PUBLIC HEARING**

Notice is hereby given that the City of Riverbank Planning Commission will conduct a public hearing to consider the projects, described below at 6:00 p.m. on Tuesday January 17, 2017, in Council Chambers 6707 Third Street, Riverbank, California.

#### **Project Description:**

**Item 3.1: CUP 02-2016 – Mobilitie, LLC** – Request to install antennas within city light poles within city rights-of-way at various locations throughout the city, including one (1) located at Oakdale Road and Patterson Road. Site ID: 9CAB009302-A. CEQA Categorical Exemption: Article 19, Section 15332 In-Fill Development Projects.

**Item 3.2: Architecture and Site Plan Review 03-2016 and Conditional Use Permit 02-2016 – Jason Huckabee** – Request for architecture and site plan review of the Del Rio Theater building located at 3300 Atchison within the Downtown Specific Plan area. The proposed project includes a remodeled restaurant/piano bar, coffee shop with drive-thru, and banquet hall, a Conditional Use Permit for serving alcohol and a future lot merge. APN: 132-009-001. CEQA Categorical Exemption: Article 19, Section 15332 In-Fill Development Projects.

#### **The City of Riverbank will hold a Public Hearing as follows:**

**Planning Commission Meeting  
January 17, 2017 at 6:00 pm  
City Hall Council Chambers - 6707 Third Street - Riverbank, California**

**ALL INTERESTED PARTIES** are invited to attend the public hearing on **January 17, 2017** at the time and place specified above to express opinions or submit evidence for or against the subject matter being considered. Written comments via e-mail to [dkenney@riverbank.org](mailto:dkenney@riverbank.org) by postal service, or hand delivered to **6707 Third Street, Suite A, Riverbank, California, 95367**, will be accepted by the Development Services Department up to 5:00 p.m. on said date. All written comments received by said time will be distributed to the Planning Commission for consideration. Oral comments will be received by the Planning Commission prior to the close of the Public Hearing on the subject matter being considered. The Planning Commission will receive all testimony prior to taking action. Testimony cannot be given over the telephone. If you challenge the City's action on these matters in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice or in written correspondence delivered to the City at, or prior to, the public hearing.

Meeting facilities are accessible to persons with disabilities. Any person requiring special assistance to participate in the meeting should notify the Administration Dept. at (209) 863-7122 or [cityclerk@riverbank.org](mailto:cityclerk@riverbank.org) at least seventy-two (72) hours prior to the meeting. For questions regarding the public hearing matter contact Donna M. Kenney, Planning & Building Manager, at (209) 863-7124; [dkenney@riverbank.org](mailto:dkenney@riverbank.org).

Any public record materials pertaining to the presentation of the subject matter being considered will be made available for review at the Development Services Counter at 6717 Third Street, Riverbank, and (if technologically possible) at <http://www.riverbank.org/Depts/planning/default.aspx> upon distribution to a majority of the Planning Commission (typically 72 hours prior to the meeting).

**CITY OF RIVERBANK  
PLANNING COMMISSION  
STAFF REPORT**

**Agenda Item No. 3.1**

**January 17, 2017**

**APPLICATION:** Conditional Use Permit No. 16-0010 – Mobilitie, LLC – Request to install, operate, and maintain communication towers, antennas, and related facilities and equipment within existing city light poles within various City rights-of-way, including one located at Oakdale and Patterson Roads in front of the O’Brien’ Shopping Center, Site ID: 9CAB009302-A.

**LOCATION:** Near 6345 Oakdale Road at Patterson Road, O’Brien’s Shopping Center Site ID: 9CAB009302-A.

**GENERAL PLAN:** C/C Community Commercial; various designations

**ZONING:** C–2 General Commercial Zoning District; various zoning districts

**ENVIRONMENTAL DETERMINATION:** The proposed project will not have a significant effect on the environment and is exempt under Section 15061 (b)(3) General Rule.

**PROJECT PLANNER:** Donna M. Kenney, Planning and Building Manager

**RECOMMENDATION:** Conditionally approve the installation, operation, and maintenance of communication towers, antennas, and related facilities and equipment within existing city light poles in various existing city rights-of-way, including one at Oakdale and Patterson Roads Site ID: 9CAB009302-A.

**ACROMYMS:** CEQA – California Environmental Quality Act  
CUP – Conditional Use Permit  
RMC – Riverbank Municipal Code

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**I. EXECUTIVE SUMMARY:**

Mobilitie, LLC (“Mobilitie”) has submitted a request to install, operate, and maintain communication towers, antennas, and related facilities and equipment within city light poles within various city rights-of-way, including one located at Oakdale and Patterson Roads. A Conditional Use Permit (“CUP”) is required as well as a right of way agreement with the city, encroachment permits, and electrical permits.

## II. BACKGROUND AND ANALYSIS:

Mobilitie” is proposing to provide a wireless infrastructure project in the City of Riverbank, within the City’s rights-of-way. Mobilitie is a public utility that is regulated by the California Public Utility Commission (“CPUC”). The CPUC has granted Mobilitie a certificate of public convenience and necessity (“CPCN”) status for authority to provide full facilities-based and resold competitive local exchange and interexchange telecommunications services.

California Public Utilities Code (“PUC”) Section 7901 allows telephone companies to place “poles, posts, piers or abutments for supporting the insulators, wires, and other necessary fixtures of their lines” in the public right-of-way. A mobile telecommunications company (also defined as a telephone company) that obtains CPCN status from the CPUC has access to the public right-of-way subject to PUC Section 7901.1, which provides that municipalities shall have the right to exercise reasonable control as to the time, place and manner in which roads, highways and waterways are accessed.

Mobilitie is constructing a new, nationwide hybrid transport network within public rights-of-way that quickly and efficiently provides high speed, high-capacity bandwidth in partnership with local jurisdictions. In addition to supporting hand-held devices, Mobilitie’s hybrid transport network can be utilized to support driverless vehicles, emergency communications systems, remote weather monitoring systems or other specific needs, where high-speed connectivity is inconsistent or lacking entirely.

The existing light pole is 26 feet in height. The addition of an omni-directional antenna to the top of the pole increases its height to 29 feet 1 inch. Smaller equipment will be visible on the sides of the poles (Attachment 2). The project is required to enter into a right of way agreement with the City per the conditions of approval and negotiate with the city the locations of future sites.

### General Plan Consistency

#### Policy Public-6.2

The City will work with local gas, communications, and electric providers to maintain and improve current levels of service and meet future demands, including the development of three phase power for industrial areas, as appropriate.

### Applicable Zoning

#### 153.336 Definitions

Alternative Tower Structure – Includes man-made trees, clock towers, bell steeples, light poles and similar alternative-design mounting structures that camouflage or conceal the presence of antennas or towers. The antennas will be installed within existing light poles.

#### 153.335 Purpose

(A)(2) – Encourage the location of towers in nonresidential areas and publicly-owned lands. The locations of chosen light poles are within city rights of way.

(A)(4) – Strongly encourage the joint use of new and existing tower sites as a primary option rather than construction of additional single-use towers. Existing light poles are the primary option for this project.

(A)(5) – Encourage users of towers and antennas to locate them, to the extent possible, in areas where the adverse impact on the community is minimal. Existing light poles are within city rights of way. The first example is along Oakdale Road within commercial uses.

(A)(6) – Encourage users of towers and antennas to configure them in a way that minimizes the adverse visual impact of the towers and antennas through careful design, siting, landscape screening, and innovative camouflaging techniques. The bulk of the antennas will be hidden within existing light poles.

(A)(10) – Protect the aesthetic entrance and corridors of the city. The bulk of the antennas will be hidden within existing light poles.

#### 153.338 General Requirements

(N) Multiple antenna/tower plan. The city strongly encourages the users of towers and antennas to submit a single application for approval of multiple towers and/or antenna sites. Applications for approval of multiple sites shall be given a priority in the review process. This application provides one light pole at Oakdale Road and Patterson Road as an example. The applicant is planning to use at least 9 other existing poles throughout the City. Therefore, a single application has been submitted for at least 10 poles, future sites to be negotiated with the city.

#### 153.341 Conditional Use Permit required.

In order to grant any use permit, the applicant must introduce evidence in support of his application sufficient to enable the Planning Commission to find that the establishment, maintenance, or operation of the use or building applied for, under the circumstances of the particular case, be detrimental to the health, safety and general welfare of the persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city. The antennas will be installed within existing light poles in city right of way. Although the public will see small attachments to the poles, there will be no additional negative impacts than the light poles currently have.

### **III. ENVIRONMENTAL DETERMINATION:**

The project meets all applicable criteria to qualify as exempt under the general rule (“Section 15061”) that CEQA applies only to projects, which have the potential for causing a significant effect on the environment. (14 Cal. Code Reg. § 15061(b)(3).) The activity is not a project subject to CEQA where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment. The project involves the installation of antenna equipment within existing city streetlight poles within the City’s existing rights-of-way. The antenna equipment is small and unobtrusive, a minor addition to the existing pole. The project is consistent with all applicable general plan policies.

#### **IV. PUBLIC NOTICE:**

The Planning Commission public hearing notice was published in the Riverbank News on January 4, 2017 and posted at City Hall North and South and city website on January 4, 2017. At the time of writing this Staff Report (January 11, 2017), the City has not received any written public comment. Written comments received by the City shall be supplied to the Commission at the day of the meeting and read into the public record.

#### **V. RECOMMENDATION:**

Find that the project is exempt from CEQA under Section 15061, and approve the Conditional Use Permit pursuant to findings and conditions contained in the attached Resolution.

- VI. ATTACHMENTS:**
1. Resolution No. 2017-001  
Exhibit A: Preliminary Site Plan
  2. Photos

Respectfully Submitted By:

*Donna M. Kenney*

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Donna M. Kenney  
Planning and Building Manager

**City of Riverbank  
Planning Commission  
Resolution No. 2017-01**

A Resolution of the Planning Commission of the City of Riverbank Approving a Conditional Use Permit for Mobilitie, LLC for the Installation of a Distributed Antenna System at Oakdale and Patterson Roads.

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**WHEREAS**, Section 153.341 of the zoning code requires a conditional use permit for the installation of such communication towers, antennas, and related facilities and equipment; and

**WHEREAS**, Mobilitie, LLC ("Mobilitie") has submitted an application to the City of Riverbank ("City") for a conditional use permit for the installation, operation, and maintenance of communication towers, antennas, and related facilities and equipment; and

**WHEREAS**, Mobilitie's conditional use permit currently seeks approval for installing such communication towers, antennas, and related facilities and equipment at one site at Oakdale and Patterson Roads and multiple future sites across the city; and

**WHEREAS**, Evidence was introduced in support of the application sufficient to enable the Planning Commission to find that the establishment, maintenance, or operation of the use or building applied for, under the circumstances of the particular case, will not be detrimental to the health, safety and general welfare of the persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city.

**THEREFORE, BE IT RESOLVED** by the Planning Commission of the City of Riverbank that it hereby approves Mobilitie's conditional use permit to allow the installation, operation, and maintenance of communication towers, antennas, and related facilities and equipment within existing light poles within existing City rights-of-way, subject to the following conditions:

1. This conditional use permit authorizes Mobilitie to install, operate, and maintain a communication tower, antennas, and related facilities and equipment within an existing light pole within the City's existing right-of-way at Oakdale and Patterson Road at the location shown in the Preliminary Site Plan attached hereto as **Exhibit A** and future sites to be negotiated with the city; and
2. Applicant shall comply with the City of Riverbank Standard Conditions as contained in the Planning Commission Resolution 2013-014 and/or receive confirmation from the Community Development Director that a specific condition or conditions does not apply to the subject project; and
3. This use permit is subject to a periodic review to monitor potential problems and for possible corrective action of the Development Services Department.

4. No variance from any City of Riverbank adopted code, policy or specification is granted or implied by the approval of this resolution.
5. That it does not result in violations to any applicable provision of any other city, state, or federal regulation, ordinance or statute.
6. Prior to installing any facilities pursuant to this use permit, applicant shall negotiate a right of way agreement with the City for the installation, operation, and maintenance of communication towers, antennas, and related facilities and equipment within the City's rights-of-way.

**BE IT FURTHER RESOLVED** that the Planning and Building Manager is authorized to enter into any such right-of-way agreement that may be negotiated and reviewed by the City Attorney.

**PASSED AND ADOPTED** by the Planning Commission of the City of Riverbank at a regular meeting held on the 17<sup>th</sup> day of January 2017, by the following vote:

**AYES:** Commissioners:

**NAYS:**

**ABSENT:**

**ABSTAIN:**

Attest:

Approved:

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Donna M. Kenney, Secretary  
Planning and Building Manager

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Anthony McKinney, Chair  
Planning Commission

**EXHIBIT A**  
**Preliminary Site Plan**

SF90XS354A/9CAB009302-A  
 37.73189908/-120.95883294  
 OAKDALE RD AND CA 108  
 RIVERBANK CITY, CA. 95367



IF YOU DIG IN ANY STATE  
 DIAL 811 FOR THE LOCAL  
 "ONE CALL CENTER" -  
 IT'S THE LAW

THE UTILITIES SHOWN HEREIN ARE FOR THE CONTRACTORS  
 CONVENIENCE ONLY. THERE MAY BE OTHER UTILITIES NOT SHOWN  
 ON THESE PLANS. THE ENGINEER/SURVEYOR ASSUMES NO  
 RESPONSIBILITY FOR THE LOCATIONS SHOWN AND IT SHALL BE  
 THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL THE UTILITIES  
 WITHIN THE LIMITS OF THE WORK. ALL DAMAGE MADE TO THE (E)  
 UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE  
 RESPONSIBILITY OF THE CONTRACTOR.

**mobilitie**  
 intelligent infrastructure

**MOBILITIE, LLC**

PROJECT NO: 9CAB009302-A  
 DRAWN BY: DWW  
 CHECKED BY: JO

B 10/13/16 UC INCORPORATED  
 A 04/28/16 FOR CLIENT REVIEW

**GENERAL NOTES**

THE FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. A  
 TECHNICIAN WILL VISIT THE SITE AS REQUIRED FOR ROUTINE  
 MAINTENANCE. THE PROJECT WILL NOT RESULT IN ANY SIGNIFICANT  
 DISTURBANCE OF EFFECT ON DRAINAGE; NO SANITARY SEWER  
 SERVICE, POTABLE WATER OR TRASH DISPOSAL IS REQUIRED AND  
 NO COMMERCIAL SIGNAGE IS (N).

**SITE INFORMATION**

SITE ID:	9CAB003902-A
CASCADE ID:	SF90XSE54A
LATITUDE:	37.73189908
LONGITUDE:	-120.95883294
CROSS STREET:	OAKDALE RD & CA 108
CITY, STATE, ZIP:	RIVERBANK, CA. 95367
COUNTY:	STANISLAUS COUNTY
JURISDICTION:	CITY OF RIVERBANK
PROPERTY OWNER:	PUBLIC RIGHT-OF-WAY
APPLICANT:	MOBILITIE, LLC 2955 RED HILL AVENUE SUITE 200 COSTA MESA, CA 92626 DAVID GRIFFITH (530) 906-1233

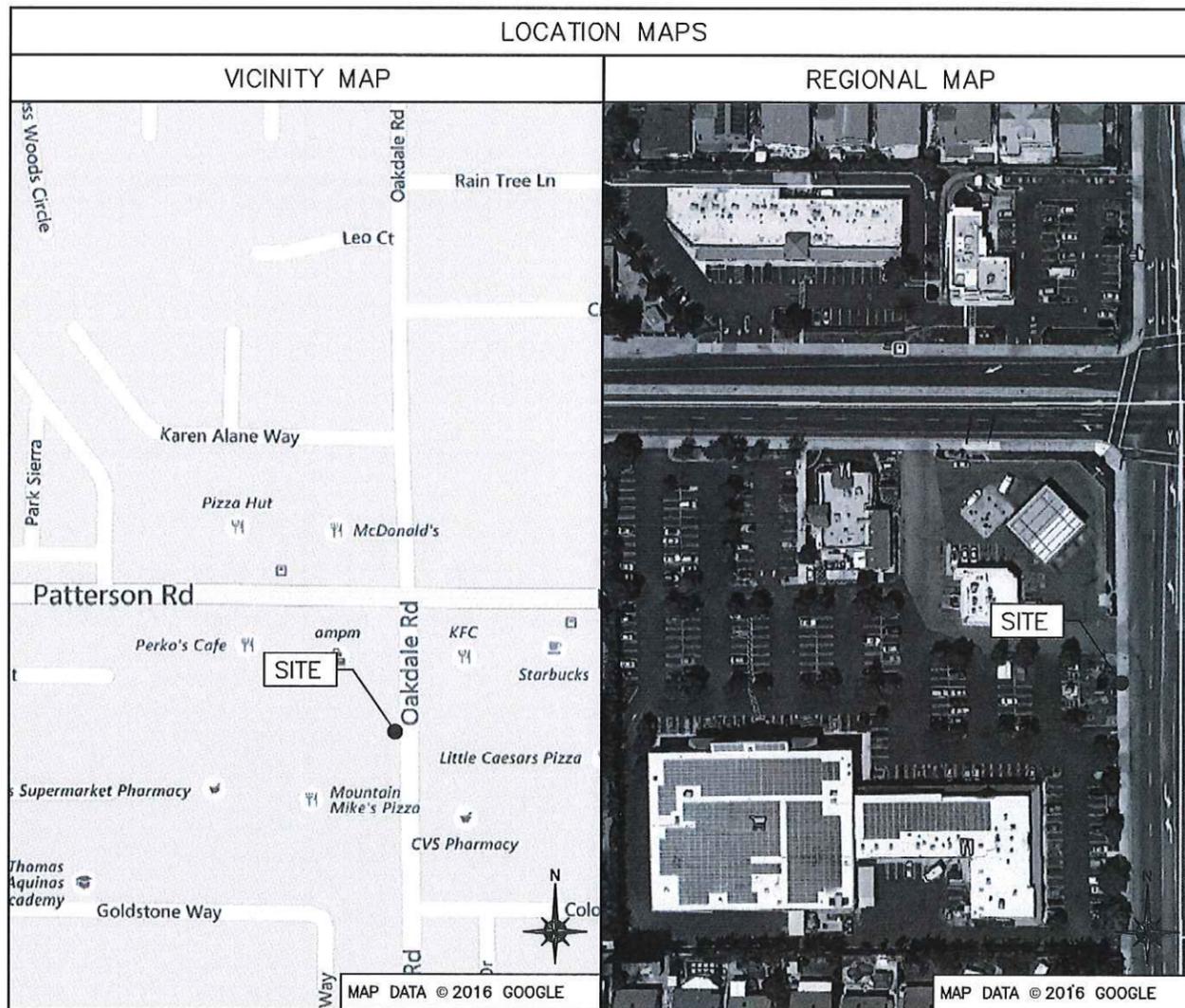
**ENGINEER**

CORTEL  
 2030 MAIN STREET  
 IRVINE, CA 92614

JASON OFFINEER  
 (619) 370-4859  
 PROJECT NUMBER: 9CAB009302-A

**DO NOT SCALE DRAWINGS**

CONTRACTORS SHALL VERIFY ALL PLANS, (E) DIMENSIONS & FIELD  
 CONDITIONS ON THE JOB SITE & SHALL IMMEDIATELY NOTIFY THE  
 ARCHITECT/ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE  
 PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.



**PROJECT DESCRIPTION**

END USER PROPOSES TO INSTALL EQUIPMENT ON AN  
 EXISTING STEEL POLE WITHIN AN EXISTING RIGHT-OF-WAY.  
 THE SCOPE WILL CONSIST OF THE FOLLOWING:

- INSTALL PROPOSED BACKHAUL TRANSPORT EQUIPMENT ON  
 AN EXISTING STEEL LIGHT POLE

**CODES**

2015 INTERNATIONAL BUILDING CODE GENERAL ORDER 95  
 2014 NATIONAL ELECTRICAL CODE GENERAL ORDER 96  
 TIA/EIA-222-G-2 OR LATEST EDITION  
 LOCAL BUILDING/PLANNING CODE

**DRAWING INDEX**

SHEET NO:	SHEET TITLE
T-1	TITLE SHEET
SP-1	EXHIBIT PHOTO & SITE PLAN
EV-1	ELEVATIONS
EV-2	ELEVATIONS
PL-1	PLUMBING & RISER DIAGRAM
EQ-1	EQUIPMENT DETAILS
EQ-2	EQUIPMENT DETAILS
EX-1	EXCAVATION DETAILS
E-1	ELECTRICAL
G-1	GROUNDING
TC-1	VEHICLE TRAFFIC CONTROL PLAN
TC-2	PEDESTRIAN TRAFFIC CONTROL PLAN
GN-1	GENERAL NOTES
GN-2	GENERAL NOTES
GN-3	GENERAL NOTES

**RECEIVED**  
 DEC 09 2016

BY: .....

IT IS A VIOLATION OF THE LAW FOR ANY  
 PERSON, UNLESS THEY ARE ACTING UNDER THE  
 DIRECTION OF A LICENSED PROFESSIONAL  
 ENGINEER, TO ALTER THIS DOCUMENT

**PRELIMINARY**

9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
 TITLE SHEET

SHEET NUMBER  
 T-1



B	10/13/16	UC INCORPORATED
A	04/28/16	FOR CLIENT REVIEW

**RECEIVED**  
DEC 09 2016

BY: .....

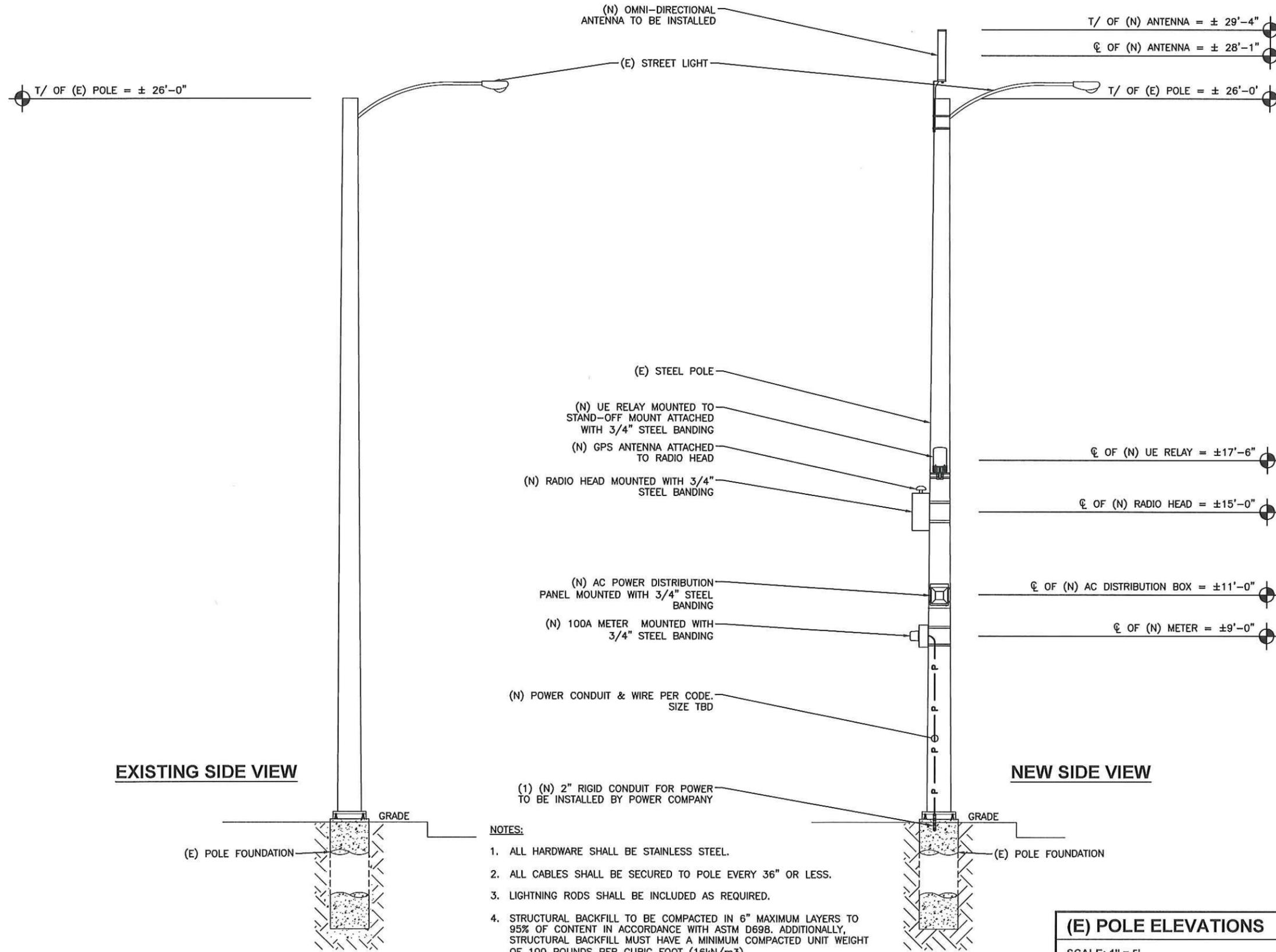
IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

**PRELIMINARY**

9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
**POLE ELEVATIONS**

SHEET NUMBER  
**EV-1**



- NOTES:**
1. ALL HARDWARE SHALL BE STAINLESS STEEL.
  2. ALL CABLES SHALL BE SECURED TO POLE EVERY 36" OR LESS.
  3. LIGHTNING RODS SHALL BE INCLUDED AS REQUIRED.
  4. STRUCTURAL BACKFILL TO BE COMPACTED IN 6" MAXIMUM LAYERS TO 95% OF CONTENT IN ACCORDANCE WITH ASTM D698. ADDITIONALLY, STRUCTURAL BACKFILL MUST HAVE A MINIMUM COMPACTED UNIT WEIGHT OF 100 POUNDS PER CUBIC FOOT (16kN/m3)

**(E) POLE ELEVATIONS**  
SCALE: 1" = 5'

B	10/13/16	UC INCORPORATED
A	04/28/16	FOR CLIENT REVIEW

RECEIVED  
DEC 09 2016

BY: .....

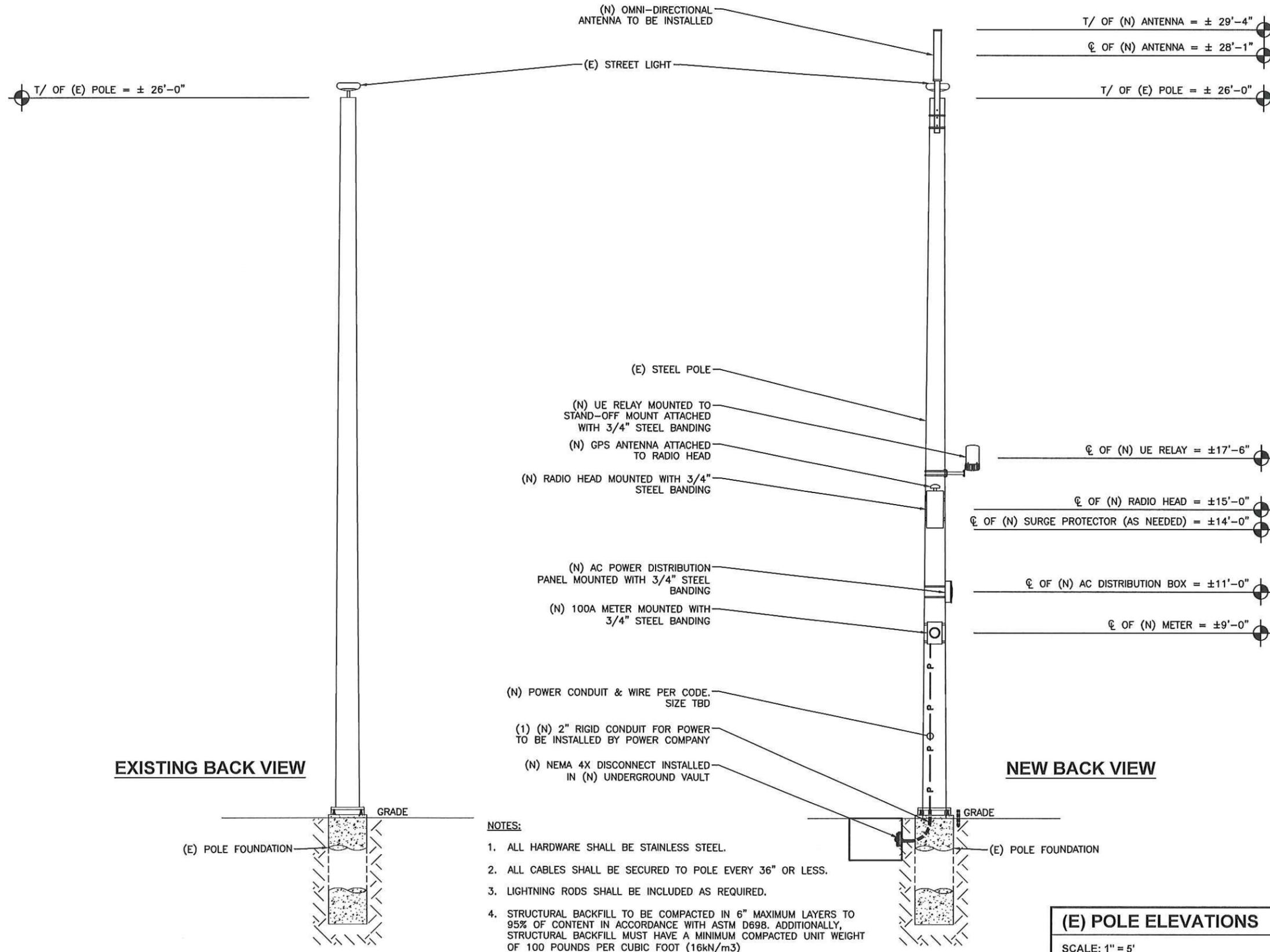
IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

PRELIMINARY

9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
POLE ELEVATIONS

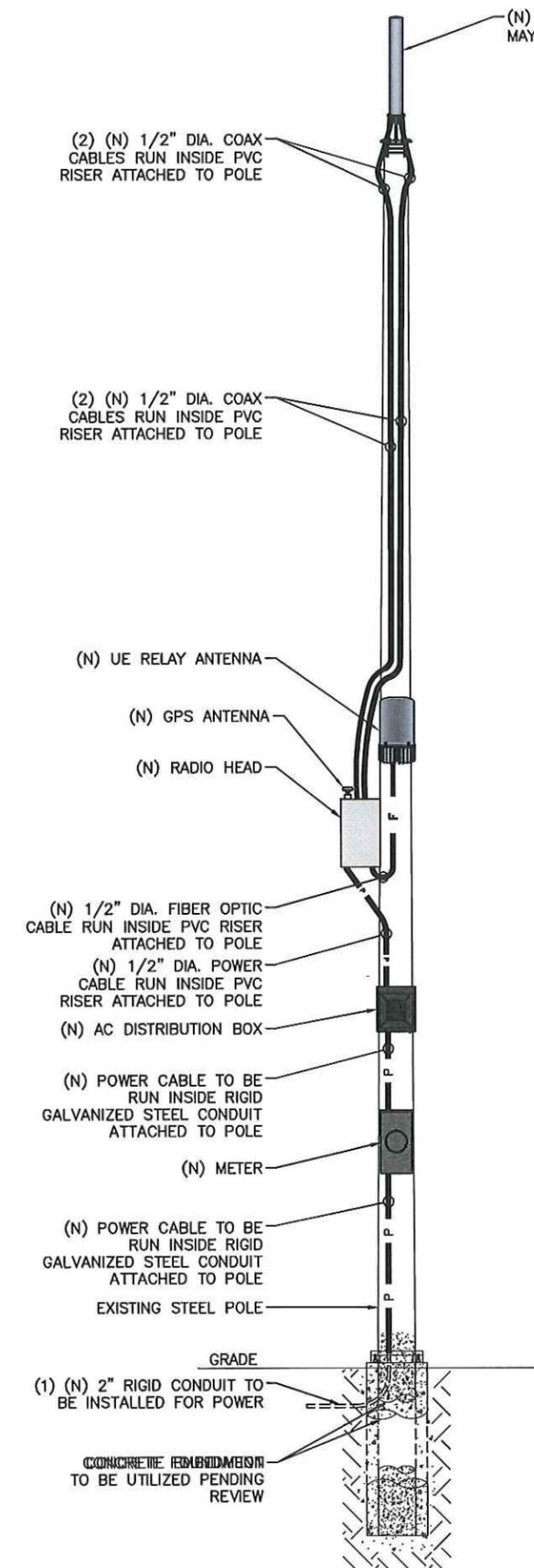
SHEET NUMBER  
EV-2



- NOTES:**
- ALL HARDWARE SHALL BE STAINLESS STEEL.
  - ALL CABLES SHALL BE SECURED TO POLE EVERY 36" OR LESS.
  - LIGHTNING RODS SHALL BE INCLUDED AS REQUIRED.
  - STRUCTURAL BACKFILL TO BE COMPACTED IN 6" MAXIMUM LAYERS TO 95% OF CONTENT IN ACCORDANCE WITH ASTM D698. ADDITIONALLY, STRUCTURAL BACKFILL MUST HAVE A MINIMUM COMPACTED UNIT WEIGHT OF 100 POUNDS PER CUBIC FOOT (16kN/m<sup>3</sup>)

**(E) POLE ELEVATIONS**  
SCALE: 1" = 5'

1



**NOTE:**  
 CABLING DIAGRAM IS FOR CLARITY OF CABLE ROUTE AND TERMINATION ONLY. CONTRACTOR SHALL INSTALL CABLES WITH MINIMAL VISUAL IMPACT ON EXISTING STEEL LIGHT POLE. SEE ELEVATION DRAWING FOR EQUIPMENT AND ANTENNA LOCATIONS.

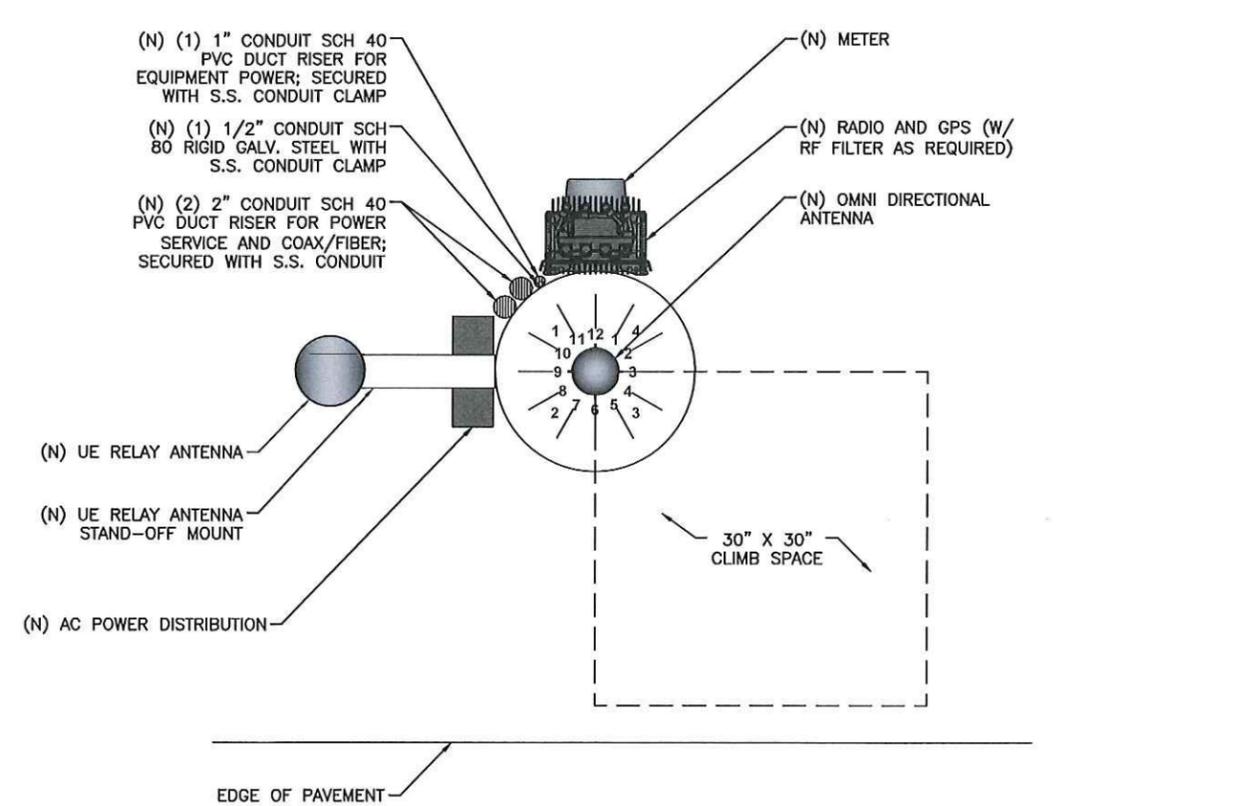
**NOTE:**  
 PROJECT SCOPE OF WORK DOES NOT INCLUDE A STRUCTURAL EVALUATION OF THIS POLE OR STRUCTURE. NEW EQUIPMENT SHOWN ON THIS PLAN HAVE NOT BEEN EVALUATED TO VERIFY THE POLE OR STRUCTURE HAS THE CAPACITY TO ADEQUATELY SUPPORT THE EQUIPMENT. PRIOR TO ANY INSTALLATION, A STRUCTURAL EVALUATION OF THE POLE OR STRUCTURE SHOULD BE PERFORMED.

- CABLING NOTES:**
- A) WOOD, CONCRETE AND EXISTING METALLIC POLES
    - I) FROM GRADE LINE TO 11'-0" ABOVE GRADE, ALL CABLES/CONDUCTORS EXCEPT GROUNDING CONDUCTOR MUST RUN IN RIGID GALVANIZED STEEL CONDUIT (RGS)
    - II) GROUNDING CONDUCTORS IN EXPOSED LOCATIONS MUST BE INSTALLED IN PVC.
    - III) IN EARTH INSTALL PVC CONDUIT FOR BACKHAUL AND ELECTRICAL SERVICE. TRANSITION TO RGS AT GRADE LINE.
    - IV) ABOVE 11'-0" ALL CABLES (POWER, ETHERNET, COAXIAL) MUST RUN IN PVC UTILITY POLE RISER.
      - (1) AT MAJOR EQUIPMENT, EXTEND UTILITY DUCT IMMEDIATELY ADJACENT TO THE EQUIPMENT. INSTALL CABLES IN THE UTILITY POLE RISER CREATING CABLE DRIP LOOPS NOT LESS THAN THE CABLE BENDING RADIUS.
      - (2) INSIDE THE UTILITY POLE RISER, UTILIZE 1/2" COAX BLOCKS WITH LAG SCREWS TO SUPPORT COAX, RADIO AND MW POWER, RF COAX, AND ETHERNET CABLES TO WITHIN 12" OF THE EQUIPMENT BEING SERVED AND ON INTERVALS NOT TO EXCEED 6'.
    - V) FOR UNDERGROUND HFC/PUBLIC BACKHAUL, ROUTE ETHERNET CABLE IN CONDUIT UP THE POLE AND ENTER THE UTILITY POLE RISER. SEAL EXPOSED END OF CONDUIT WITH A CABLE TERMINATION FITTING.
    - VI) BY APPROVAL IN SELECT CASES LIQUID-TIGHT FLEXIBLE METALLIC CONDUIT (LFMC) MAY BE USED IN LENGTHS NOT TO EXCEED 36" TO EXTEND THE ELECTRICAL SERVICE CONDUIT TO THE AC DISTRIBUTION BOX. EXAMPLE: UTILITY-REQUIRED DISCONNECT ON POLE W/ AC DISTRIBUTION BOX ON OPPOSITE SIDE OF POLE.
  - B) NEW METALLIC POLES
    - I) PROCURE NEW POLES WITH SUITABLE HAND HOLES SUCH THAT HAND HOLES EXIST AT ALL EQUIPMENT LOCATIONS.
      - (1) WITH CLIENT APPROVAL IN SELECT CASES TO FACILITATE IMPROVED APPEARANCE, 1/2" COAXIAL CABLES MAY BE "SUPERFLEX" IN LIEU OF LDF-4.
    - II) WHERE POSSIBLE, INSTALL POLE BASE SUCH THAT THE ELECTRICAL FEED AND BACKHAUL (IF UNDERGROUND) CIRCUIT ENTER THE POLE THROUGH THE POLE BASE. IF A DISCONNECTING MEANS SEPARATE FROM THE AC DISTRIBUTION BOX IS REQUIRED BY JURISDICTION OR UTILITY, WITH APPROVAL IN SELECT CASES LIQUID-TIGHT FLEXIBLE METALLIC CONDUIT (LFMC) MAY BE USED IN LENGTHS NOT TO EXCEED 36" TO EXTEND THE ELECTRICAL SERVICE CONDUIT TO THE AC DISTRIBUTION BOX.

**PLUMBING DIAGRAM**  
 SCALE: NOT TO SCALE

BAND 41 (2500MHz) EQUIPMENT CHART						
QTY.	DESCRIPTION	MANUFACTURER	MODEL NUMBER	CABLE LENGTH	DIMENSIONS (HxWxD)	WEIGHT
1	MOUNTED ANTENNA	ALPHA WIRELESS	AW3477	12'±	20.9" X 4.5" DIAMETER	8.8 LBS
1	UE RELAY	AIRSPAN	iR460	2'±	12.99" X 7.87" DIAMETER	9.9 LBS
1	GPS ANTENNA	TALLYSMAN	TW3012	-	0.8" X 2.6" DIAMETER	0.3 LBS
1	RADIO (W/ FILTER AS REQUIRED)	AIRSPAN	AH1000D	4'-6"±	18.9" X 9.6" X 7.48"	38.14 LBS
1	AC DISTRIBUTION	RAYCAP	RSTAC-3111-P-120	1"±	13.13" X 11.38" X 4.38"	14 LBS
1	DISCONNECT	TBD	TYPE-3R	1'±	9.9" X 8.8" X 4.5"	TBD
1	METER	MILBANK	U7021-RL-TG	162'±	15.5" X 8" X 4.33"	TBD

**BILL OF MATERIALS**  
 SCALE: NOT TO SCALE



**RISER DIAGRAM**  
 SCALE: NOT TO SCALE



**MOBILITIE, LLC**

PROJECT NO: 9CAB009302-A  
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 CHECKED BY: JO

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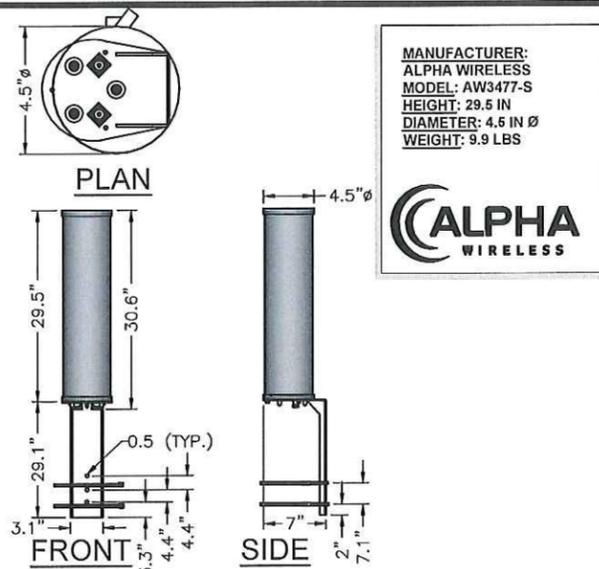
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**PRELIMINARY**

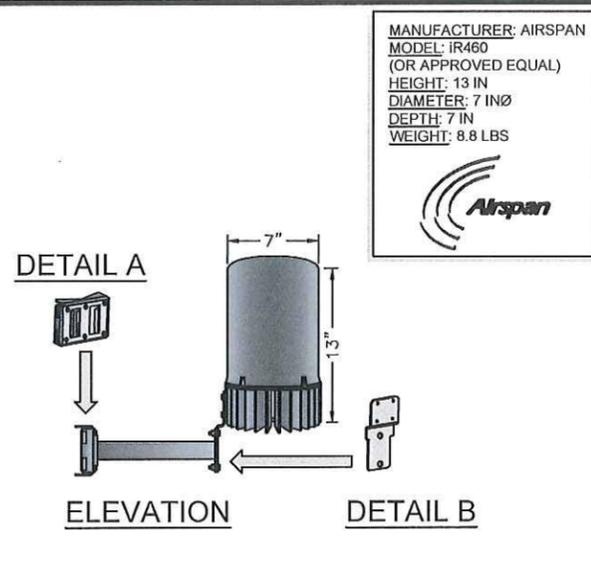
9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
**PLUMBING & RISER DIAGRAM**

SHEET NUMBER  
**PL-1**

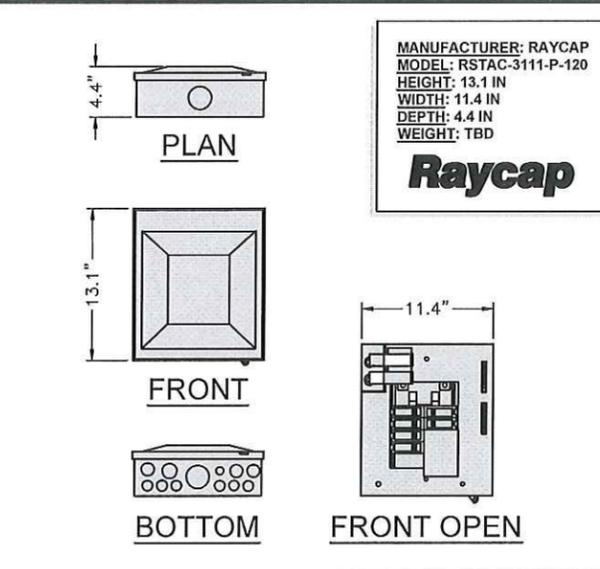


ALPHA AW3477 OMNI (B41) SCALE N.T.S. 1

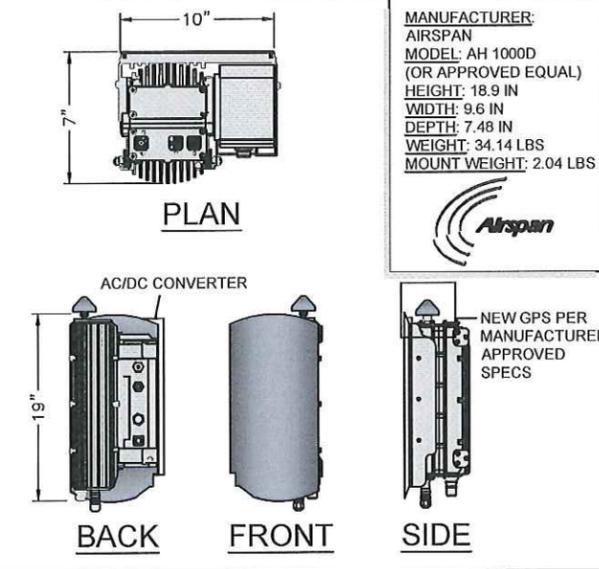


IR460 UE RELAY SCALE N.T.S. 4

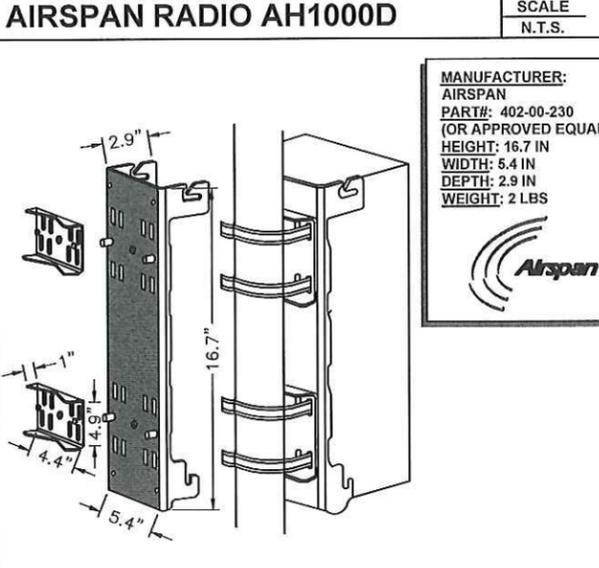
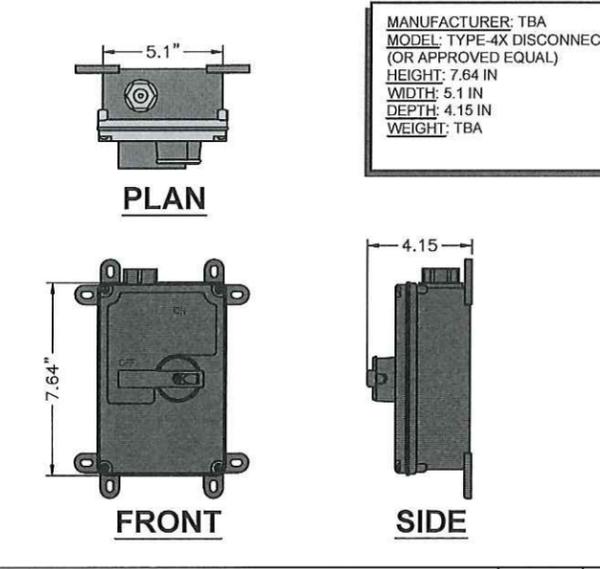
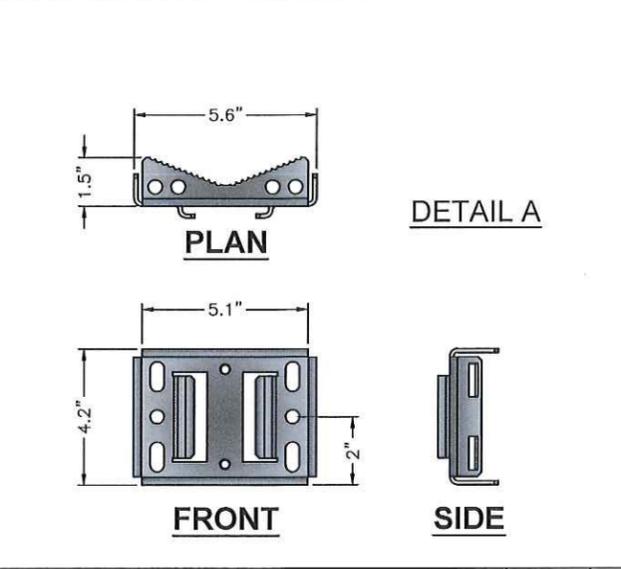
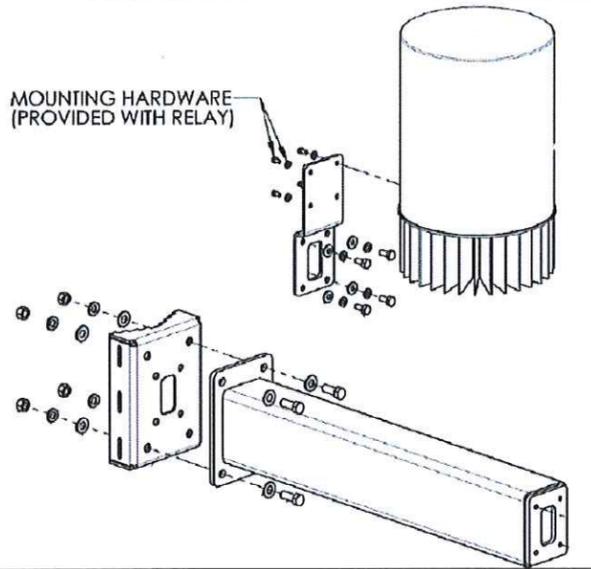
NOT USED SCALE N.T.S. 7



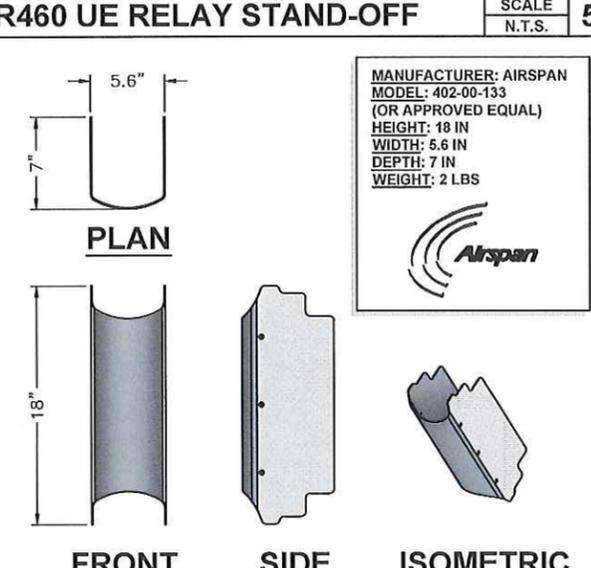
RAYCAP RSTAC-3111-P-120 SCALE N.T.S. 10



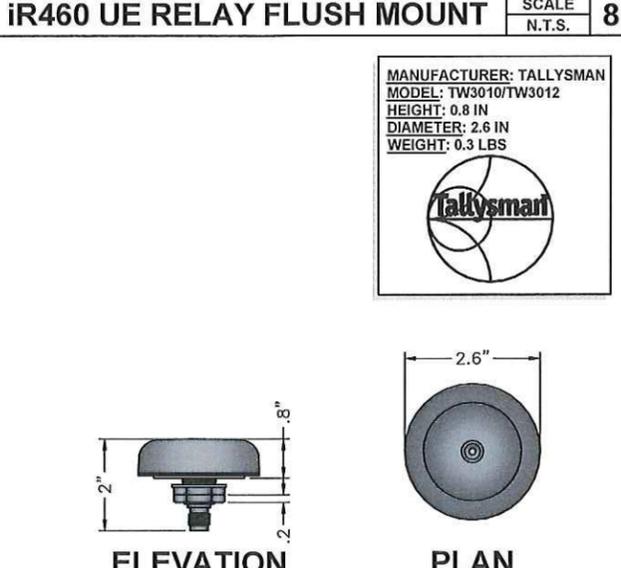
AIRSPAN RADIO AH1000D SCALE N.T.S.



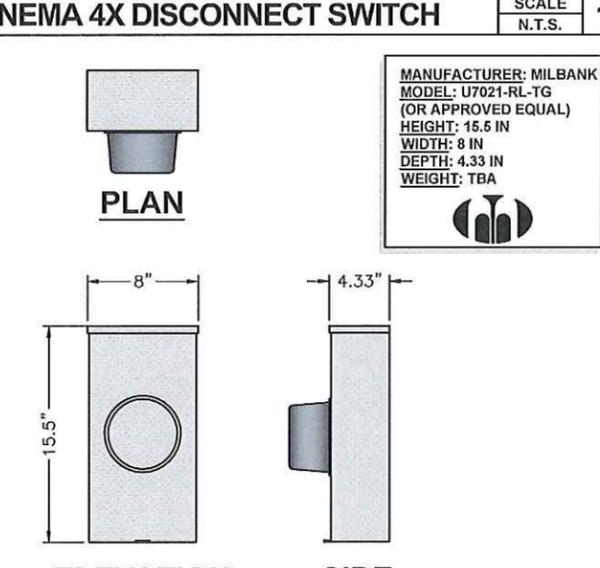
AIRSPAN RADIO MOUNT SCALE N.T.S. 3



AIRSPAN RADIO SUN SHIELD SCALE N.T.S. 6



TALLYSMAN GPS ANTENNA SCALE N.T.S. 9



MILBANK METER SOCKET SCALE N.T.S. 12



MOBILITIE, LLC

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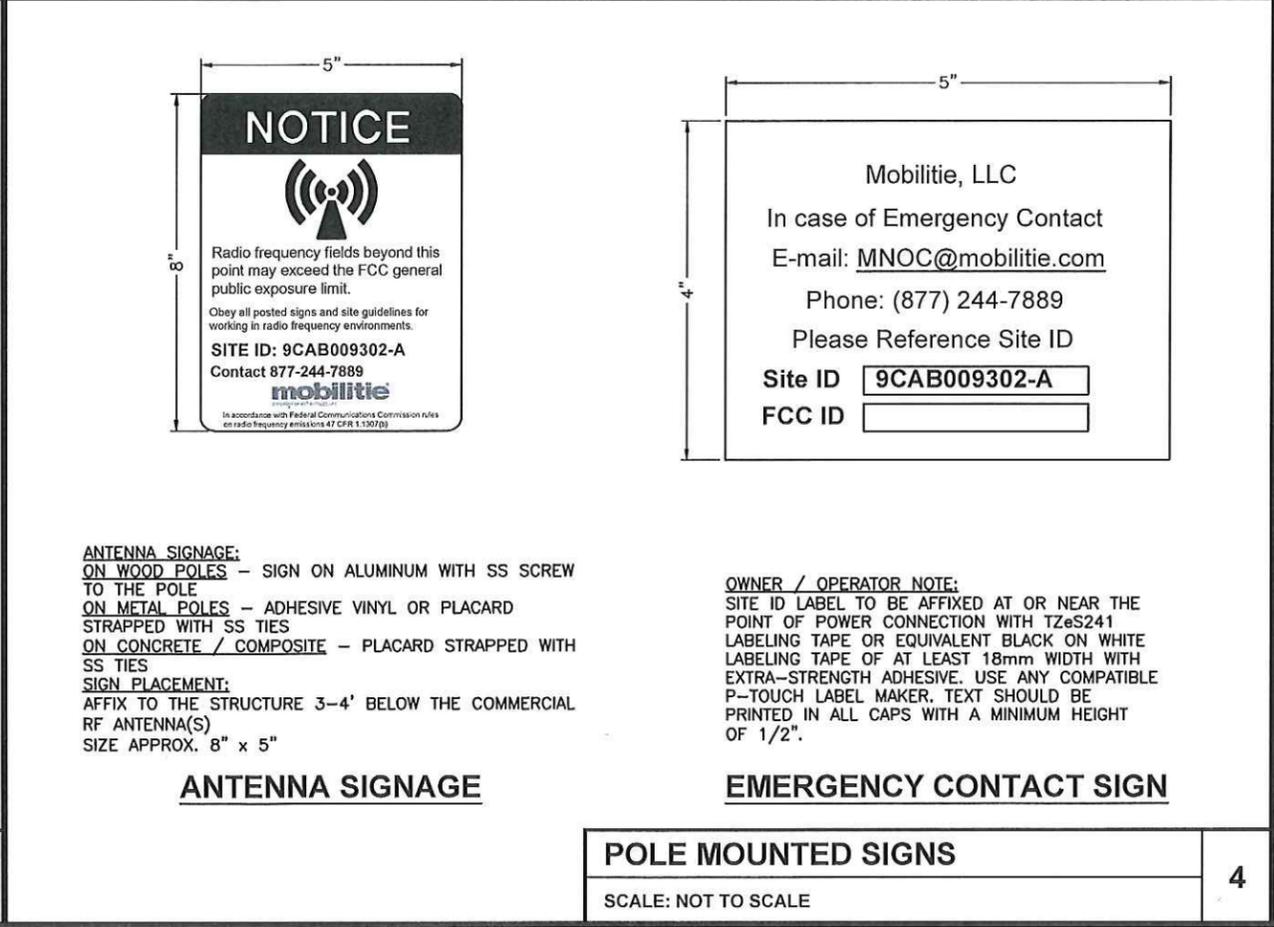
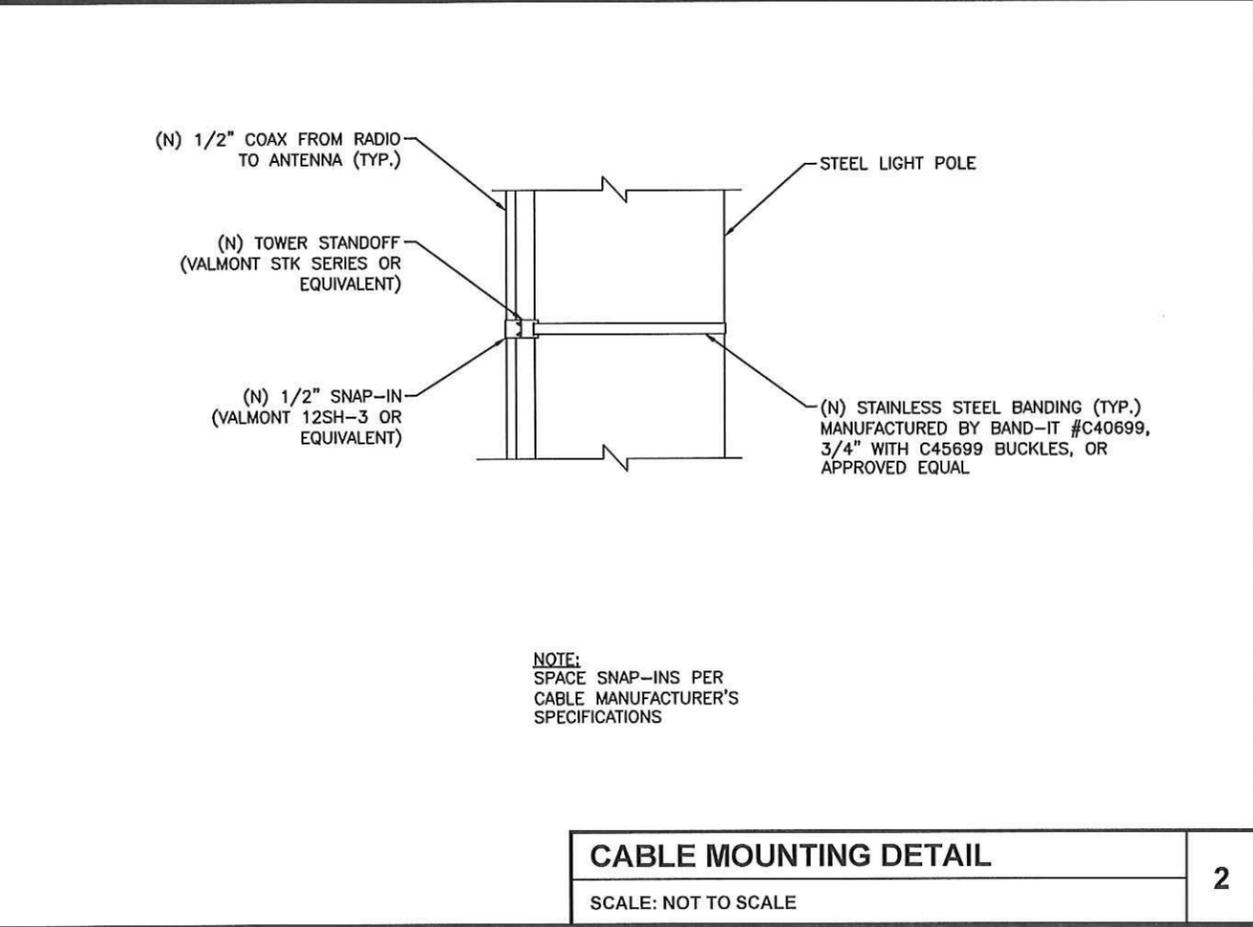
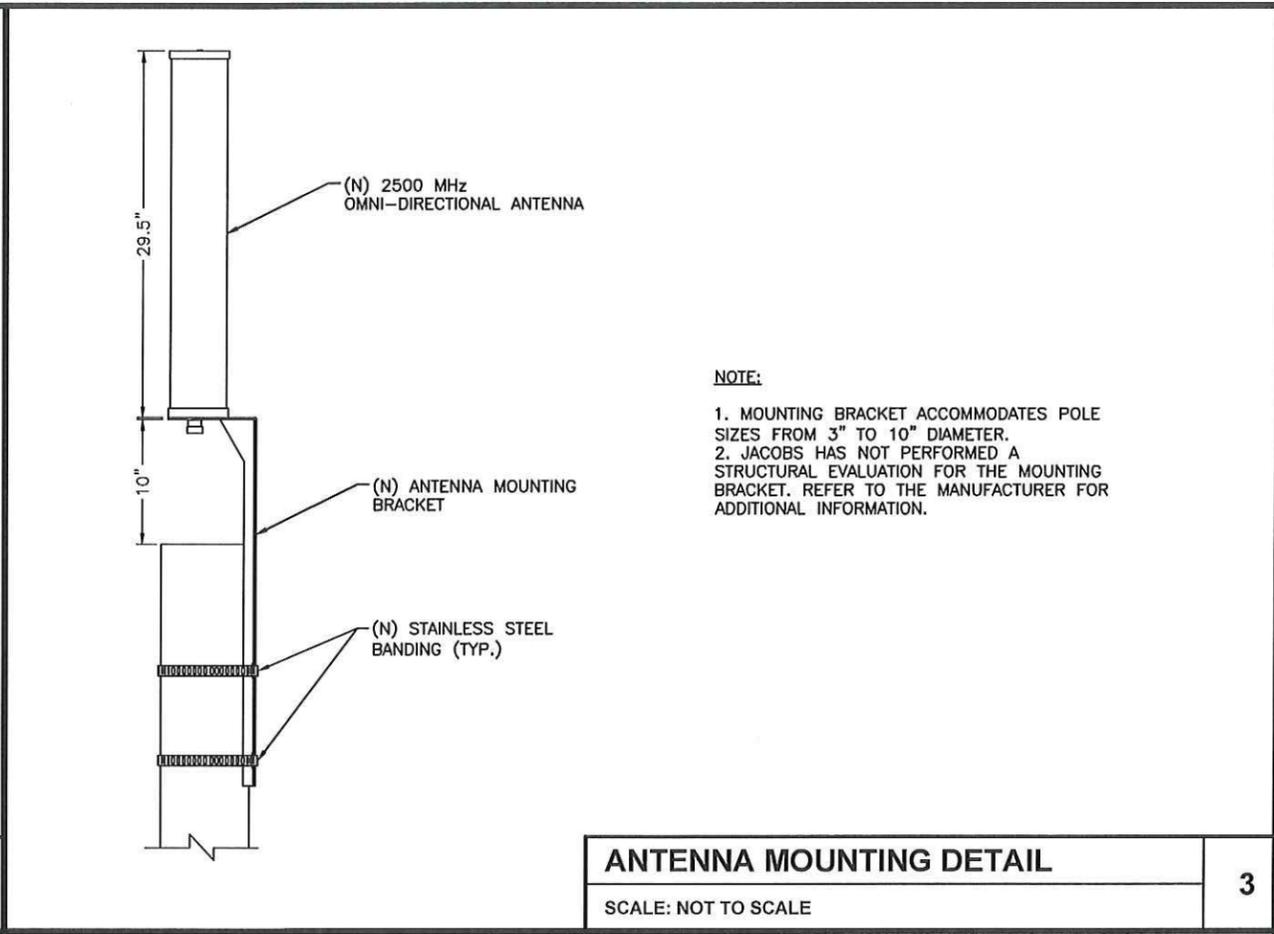
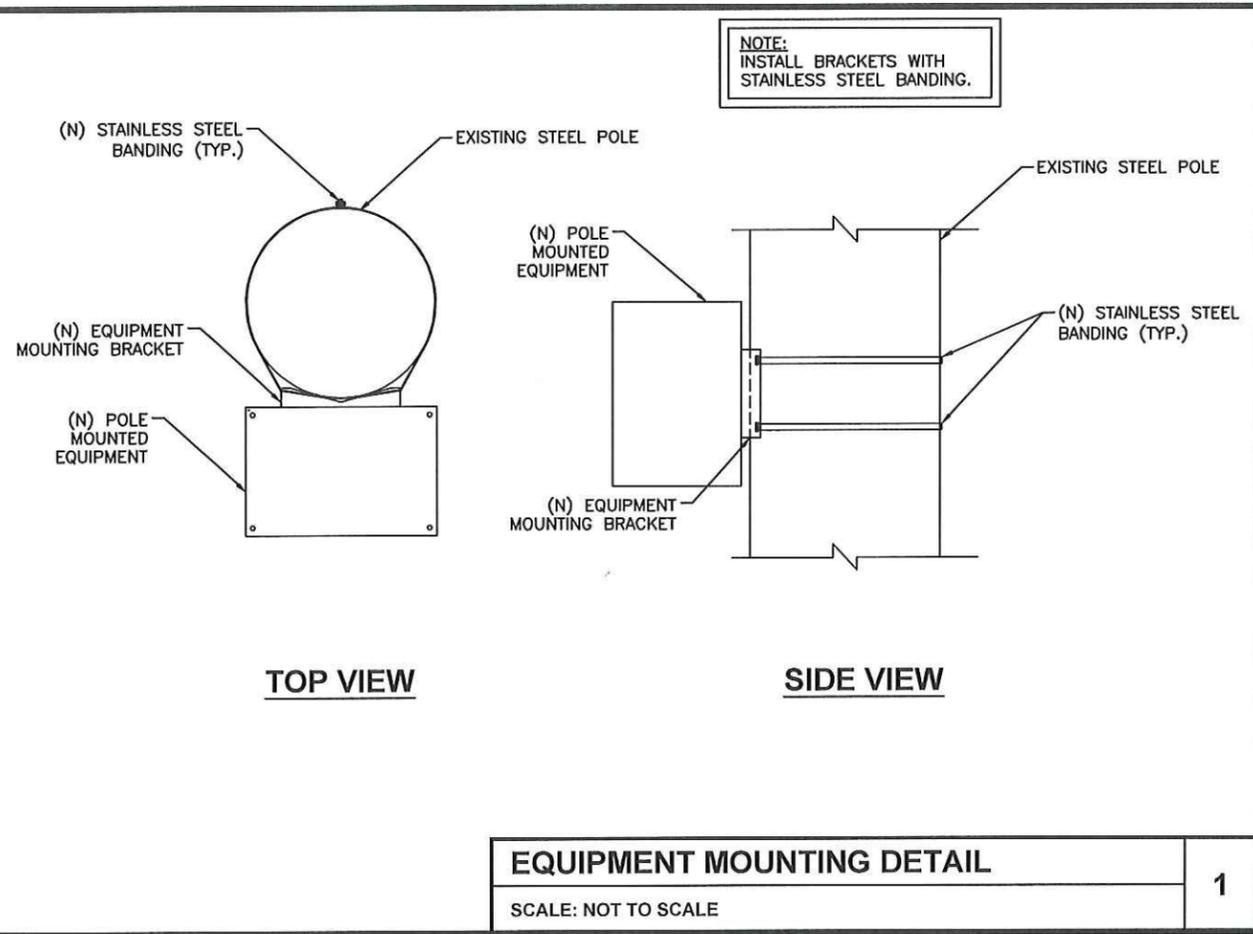
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PRELIMINARY

9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
 EQUIPMENT DETAILS

SHEET NUMBER  
 EQ-1



**mobilitie**  
intelligent infrastructure

**MOBILITIE, LLC**

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**PRELIMINARY**

9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 28'-0" STEEL LIGHT POLE

SHEET TITLE  
EQUIPMENT DETAILS

SHEET NUMBER  
EQ-2

PROJECT NO: 9CAB009302-A  
DRAWN BY: DWW  
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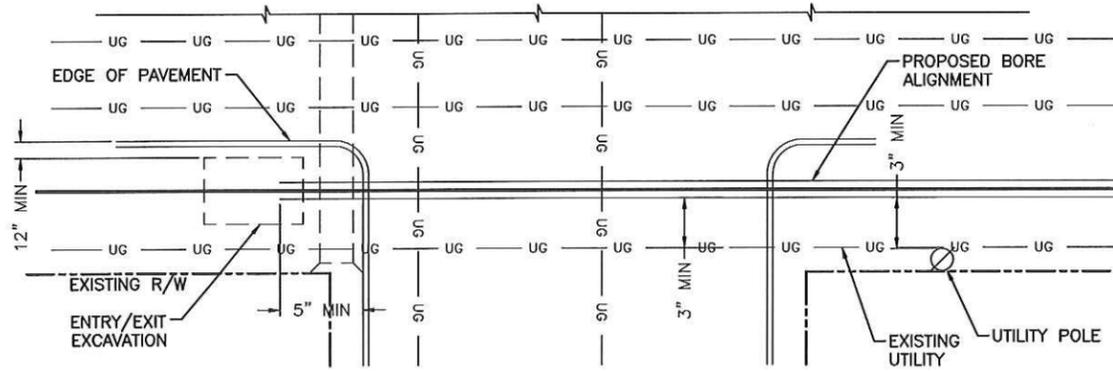
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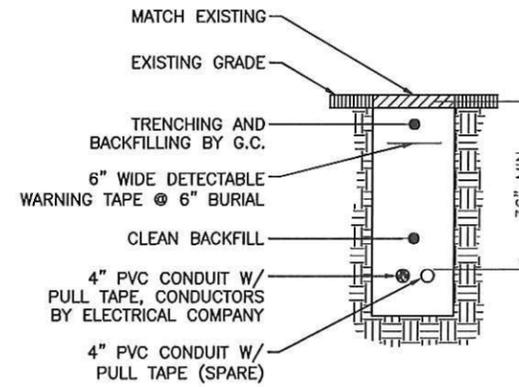
9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
EXCAVATION DETAILS

SHEET NUMBER  
EX-1



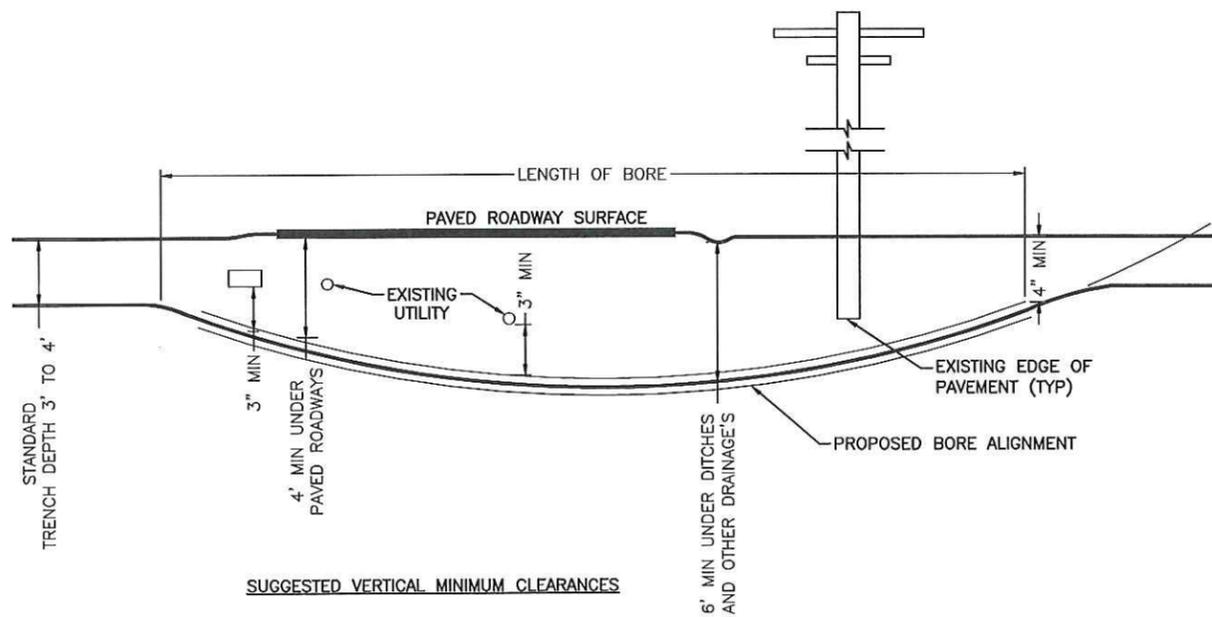
SUGGESTED HORIZONTAL MINIMUM CLEARANCES



**TRENCH SECTION DETAIL**

SCALE: NOT TO SCALE

2



SUGGESTED VERTICAL MINIMUM CLEARANCES

**DIRECTIONAL BORING DETAILS**

SCALE: NOT TO SCALE

1

**NOT USED**

SCALE: NOT TO SCALE

3

PROJECT NO: 9CAB009302-A  
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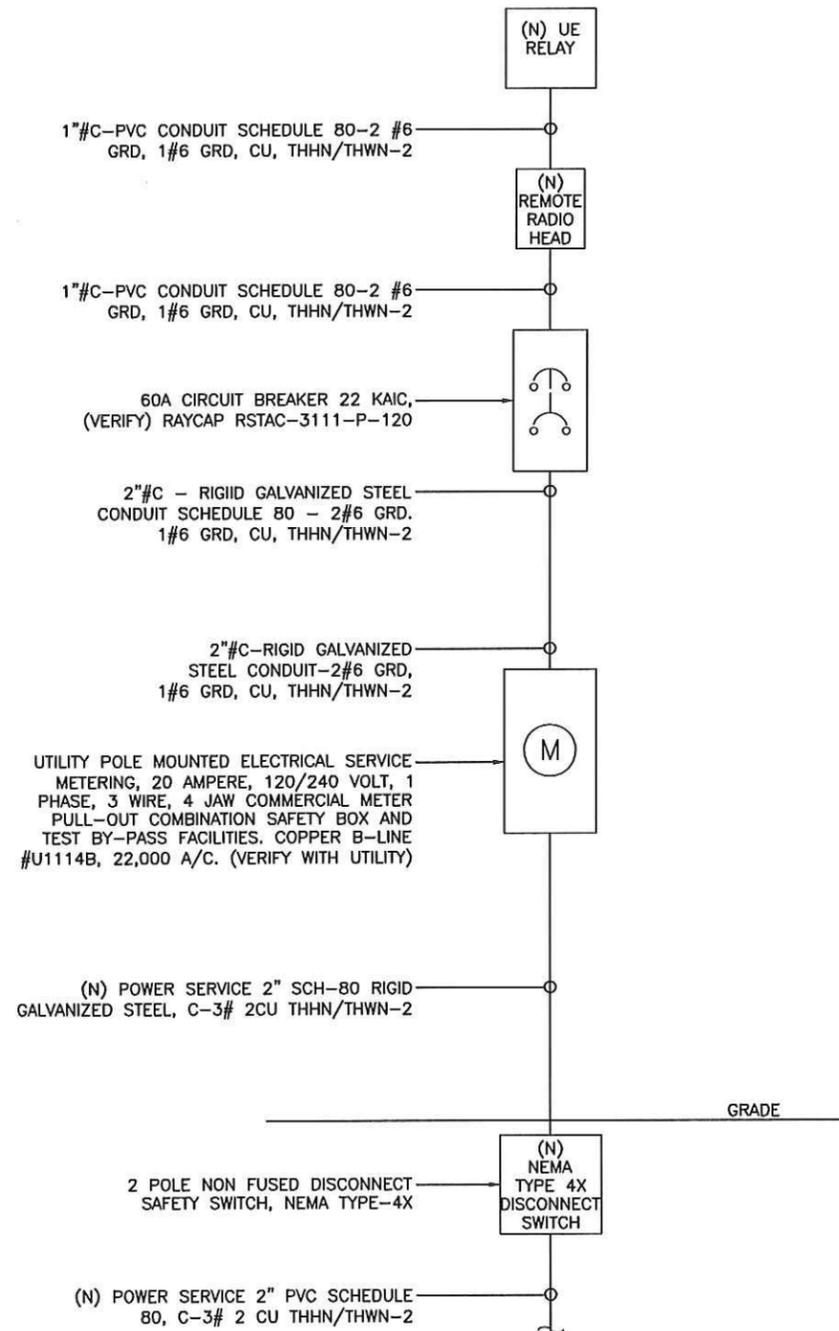
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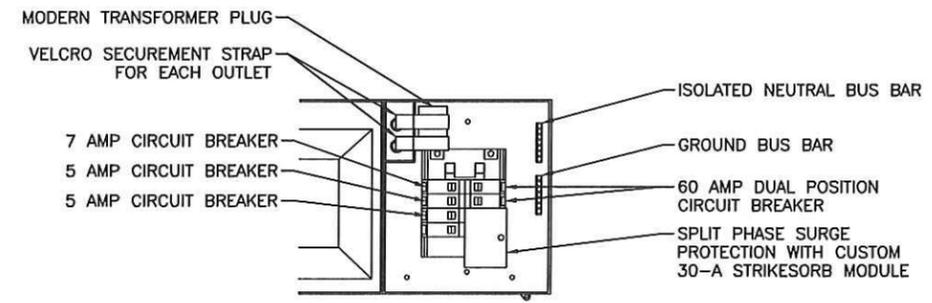
9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
**ELECTRICAL**

SHEET NUMBER  
**E-1**



**ONE-LINE DIAGRAM**  
SCALE: NOT TO SCALE 1



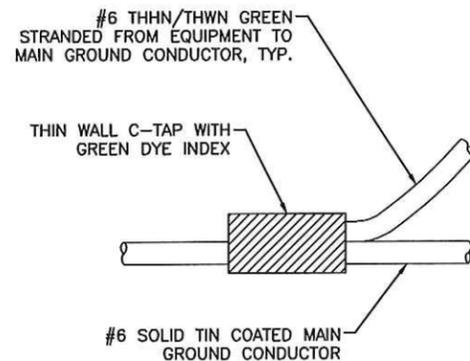
**BREAKER SCHEDULE**  
SCALE: NOT TO SCALE 2

- NOTES:**
1. NOMINAL POWER IS CALCULATED AS 80% OF OEM DOCUMENTED MAXIMUM POWER.
  2. CALCULATIONS FOR UE W/ NOKIA DO NOT NEED TO INCLUDE THE POWER FOR THE UE ANTENNA AS IT IS INCLUDED IN THE MAX POWER FIGURE. CALCULATIONS FOR UE W/ AIRSPAN MUST INCLUDE UE AS IT IS NOT INCLUDED
  3. KVA IS CALCULATED FROM THE CONSUMPTION VALUE ASSUMING A PF=1. MAXIMUM POWER WAS USED FOR KVA. WHERE MAXIMUM WAS NOTED BY THE OEM THE QUOTED FIGURE WAS USED. WHERE AVERAGE/NOMINAL POWER WAS NOTED BY THE OEM MAXIMUM POWER WAS CALCULATED BY INCREASING AVERAGE/NOMINAL POWER BY A FACTOR OF 50%
  4. COST PER KW PROVIDED BY BRAIN KOOYMAN

AIRSPAN SCENARIO 1 AH1000D LOW POWER RADIO AND UE BACKHAUL									
UNIT	SUB DESCRIPTION	MAX POWER	NOMINAL POWER	AVERAGE POWER (W)	CONS. (W)	KVA	KWH/YR	\$/YR	\$/MO
AH1000D	B41 LOW	N/A	N/A	130	130	0.195	1138.8	\$124.81	\$10.40
AIRSPAN UE RELAY	IR460-SPB-ST1-P-0	N/A	N/A	20	20	0.02	175.2	\$19.20	\$1.60
TOTAL					150	0.215	1314	\$144.01	\$12.00

**LOAD CALCULATIONS**  
SCALE: NOT TO SCALE 3

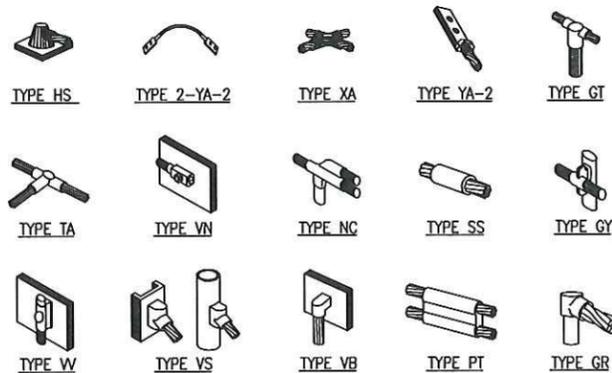
NOTE:  
CONTRACTOR TO SURROUND COMPLETED CONNECTION WITH HEAT-SHRINK TUBING TO ENSURE WEATHER PROOF CONNECTION



**C-TAP DETAIL**

SCALE: NOT TO SCALE

1



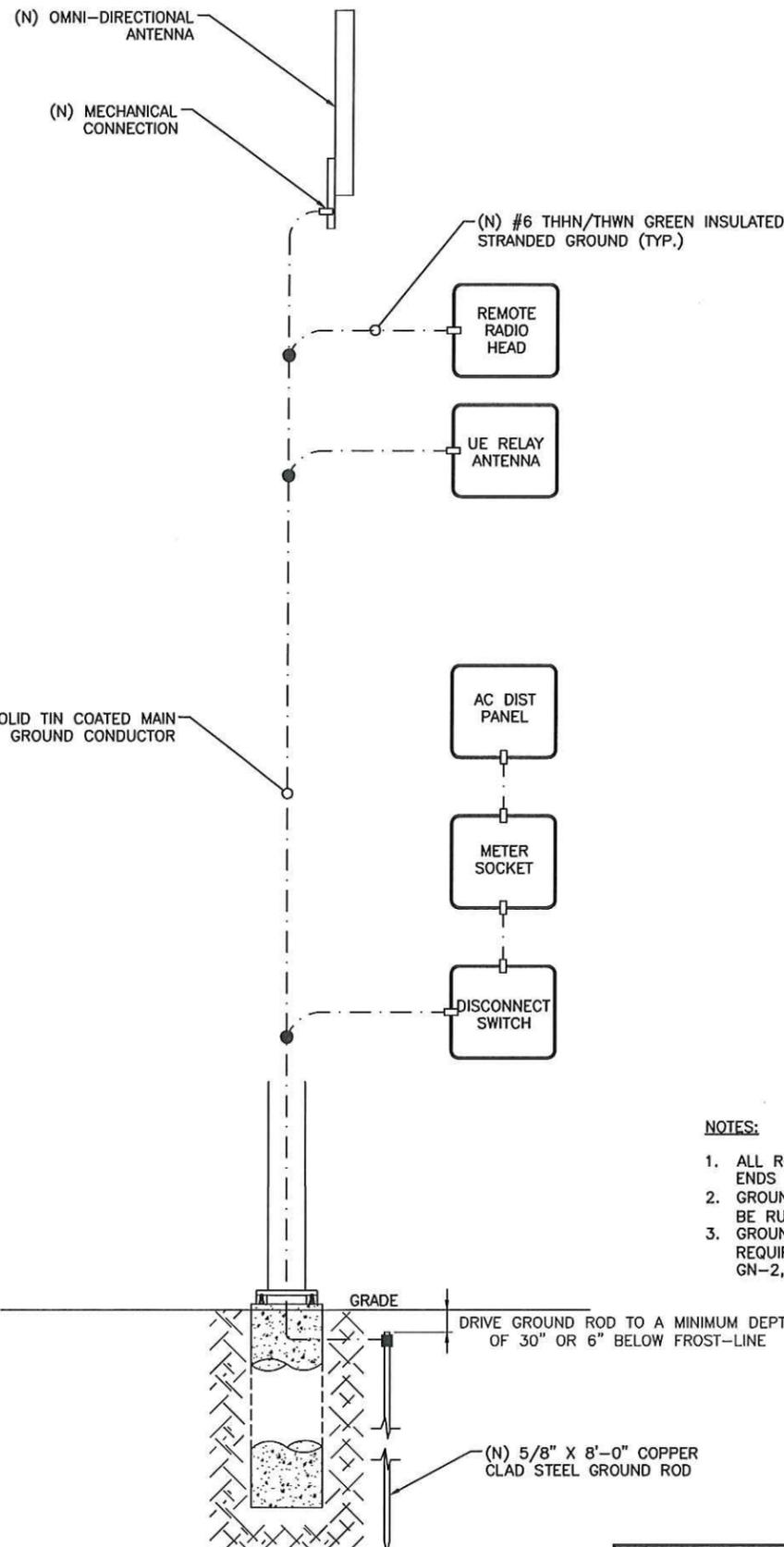
NOTE:

ERICO EXOTHERMIC "MOLD TYPES" SHOWN HERE ARE EXAMPLES. CONSULT WITH PROJECT MANAGER FOR SPECIFIC MOLDS TO BE USED FOR THIS PROJECT.

**WELD CONNECTION DETAILS**

SCALE: NOT TO SCALE

2



**LEGEND**

- CADWELD CONNECTION
- MECHANICAL CONNECTION
- COMPRESSION CONNECTION

NOTE:  
GROUNDING RISER FOR DIAGRAMMATIC PURPOSES ONLY. SEE ELEVATION DRAWING FOR EQUIPMENT AND ANTENNA LOCATIONS.

NOTES:

1. ALL RGS TO BE GROUNDED AT BOTH ENDS USING GROUNDING BUSHINGS
2. GROUND WIRE BELOW 11' AGL TO BE RUN IN 1/2" SCHEDULE 40 PVC
3. GROUNDING ELECTRODE SYSTEM REQUIRES "BOND" SEE NOTES ON GN-2, E-1 AND G-1

**GROUNDING RISER DIAGRAM**

SCALE: NOT TO SCALE

3



**MOBILITIE, LLC**

PROJECT NO: 9CAB009302-A  
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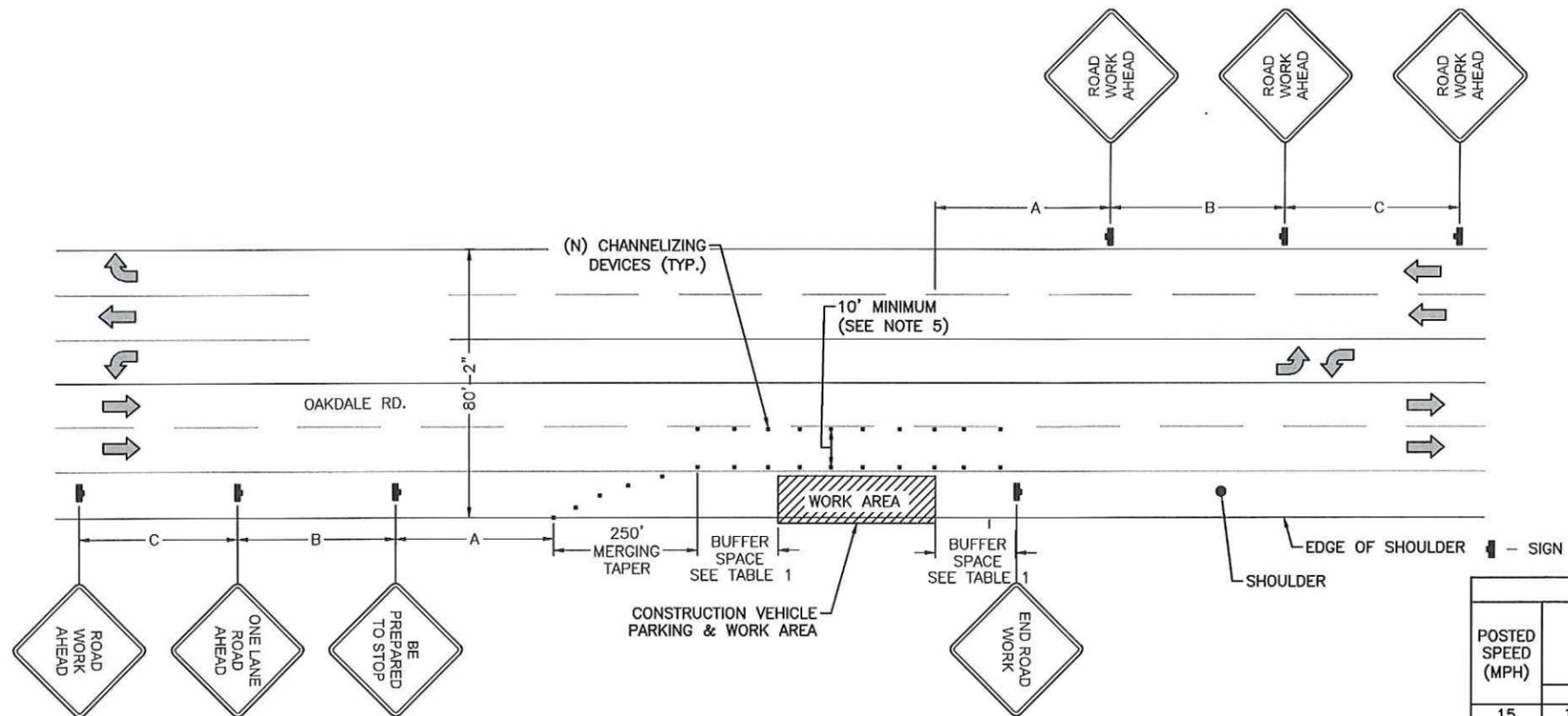
9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
GROUNDING DETAILS

SHEET NUMBER  
G-1

**PLAN NOTES:**

1. PLANS DEPICTED ARE GENERAL GUIDELINES FOR TEMPORARY TRAFFIC CONTROL PLANS (TCP) TO INCLUDE PEDESTRIAN AND WORKER SAFETY. CONTRACTOR IS REQUIRED TO HAVE PREPARED A SITE-SPECIFIC TCP FOR REVIEW AND APPROVAL BY THE HIGHWAY AUTHORITY HAVING JURISDICTION. IF REQUIRED, THE FIRM PREPARING THE TCP SHALL BE AUTHORIZED OR CERTIFIED BY THE AUTHORITY HAVING JURISDICTION.
2. EXTEND CHANNELIZATION DEVICES INTO SHOULDER WHERE APPLICABLE.
3. DISTANCES AS INDICATED IN TABLE 1 SHOULD BE INCREASED FOR CONDITIONS THAT WOULD AFFECT STOPPING. DISTANCE SUCH AS DOWNGRADES OR LIMITED SIGHT DISTANCES. DISTANCES CAN BE DECREASED FOR LOW-SPEED (RESIDENTIAL) AREAS WITH APPROVAL BY THE AUTHORITY HAVING JURISDICTION. NIGHT-TIME WORK IS PROHIBITED UNLESS IT IS REQUIRED AS A CONDITION OF APPROVAL BY THE HIGHWAY AND LOCAL AUTHORITY HAVING JURISDICTION.
4. SHOULDER TAPERS SHOULD BE 1/3 OF THE ON-STREET TAPER LENGTH.
5. MAINTAIN A MINIMUM LANE WIDTH OF 10'.



POSTED SPEED (MPH)	DISTANCE BETWEEN SIGNS			TAPER	BUFFER
	A	B	C	L (SEE NOTE)	
15	100'	100'	100'	45'	100'
20	100'	100'	100'	80'	115'
25	100'	100'	100'	125'	155'
30	200'	200'	200'	180'	200'
35	200'	200'	200'	245'	250'
40	350'	350'	350'	320'	305'
45	350'	350'	350'	540'	360'
50	500'	500'	500'	600'	425'
55	500'	500'	500'	660'	495'
60	500'	500'	500'	720'	570'
65	500'	500'	500'	780'	645'

- NOTES:
- A) DISTANCES IN FEET UNLESS OTHERWISE NOTED.
  - B) CONTRACTOR TO VERIFY (E) SPEED LIMIT.
  - C) DISTANCES SHOWN ARE NOT VALID FOR LIMITED ACCESS HIGHWAYS. CONSULT STATE DOT MANUAL FOR DISTANCES.
  - D) ADJUST DISTANCES TO COMPLY WITH REQUIREMENT OF THE STATE OR LOCAL HIGHWAY AUTHORITY HAVING JURISDICTION. SEE NOTE 1, SHEET 6.1.
  - E) TAPER LENGTHS SHOWN BASED ON 12' LANE WIDTH. SEE NOTE 18, SHEET 6.1.

**TRAFFIC CONTROL PLAN - SHOULDER WORK**

SCALE: NOT TO SCALE

1



**MOBILITIE, LLC**

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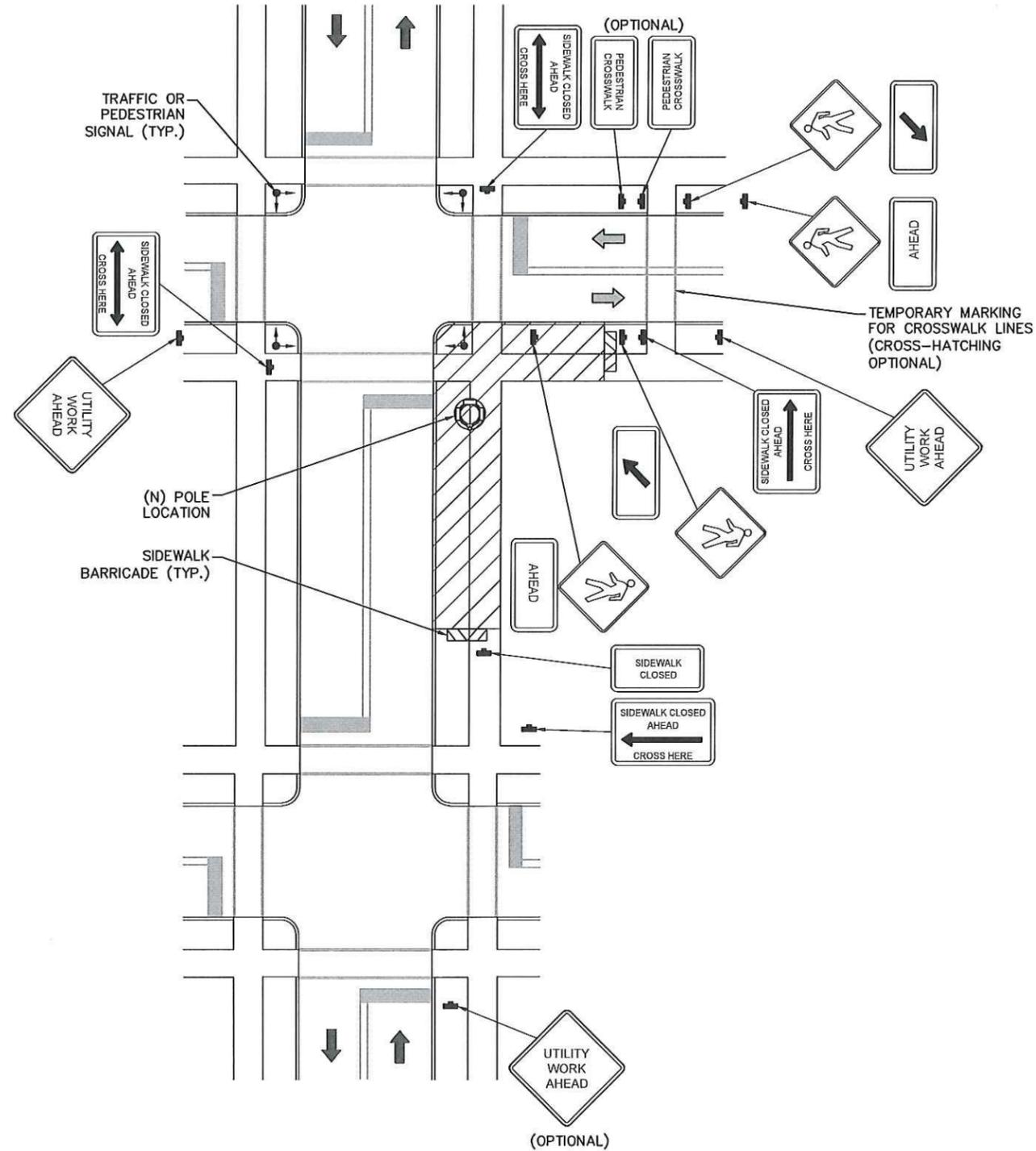
9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 28'-0" STEEL LIGHT POLE

SHEET TITLE  
**TRAFFIC CONTROL NOTES**

SHEET NUMBER  
**TC-1**

**TRAFFIC CONTROL GENERAL NOTES**

- ALL TEMPORARY TRAFFIC CONTROL SIGNAGE, LAYOUTS AND PROCEDURES SHALL COMPLY WITH LOCAL JURISDICTIONAL REQUIREMENTS AND MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), LATEST EDITION, WHICHEVER IS MORE STRINGENT.
- PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE IN PLACE.
- TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. SHALL BE PLACED AS SHOWN ON PLANS. SIGNS SHALL NOT BE PLACED WITHOUT ACTUAL LANE CLOSURES AND SHALL BE IMMEDIATELY REMOVED UPON REMOVAL OF THE CLOSURES.
- SELECTION, PLACEMENT, MAINTENANCE, AND PROTECTION OF TRAFFIC, PEDESTRIANS, AND WORKERS SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) – PART VI "TEMPORARY TRAFFIC CONTROL", AND LOCAL JURISDICTIONAL REQUIREMENTS UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS, AND SHALL BE APPROVED BY THE APPROPRIATE HIGHWAY AUTHORITY HAVING JURISDICTION.
- ADVANCE WARNING SIGNS, DISTANCES, AND TAPER LENGTHS MAY BE EXTENDED TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY AND FOR ACTUAL TRAFFIC SPEEDS IF IN EXCESS OF POSTED SPEED LIMITS.
- TAPERS SHALL BE LOCATED TO MAXIMIZE THE VISIBILITY OF THEIR TOTAL LENGTH.
- CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON THE (E) TRAFFIC SIGNAL SYSTEMS SHALL BE BAGGED OR COVERED.
- ALL (E) ROAD SIGNS, PAVEMENT MARKINGS AND/OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE (N) TRAFFIC CONTROL PLAN SHALL BE COVERED, REMOVED, OR RELOCATED. ALL TRAFFIC CONTROL DEVICES SHALL BE RESTORED TO MATCH PRE-CONSTRUCTION CONDITION AFTER COMPLETION OF WORK.
- CONTRACTOR SHALL CONTACT LOCAL AUTHORITY HAVING HIGHWAY JURISDICTION AND PROVIDE ADDITIONAL "FLAGMEN" OR POLICE SUPERVISION, IF REQUIRED.
- ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY SHALL BE BACKFILLED AND PLACED ON A MINIMUM 6H:1V SLOPE PRIOR TO END OF EACH WORK DAY. OTHER EXCAVATED AREAS WITHIN THE CLEAR ZONE ARE TO BE EITHER BACKFILLED OR PRECAST CONCRETE CURB BARRIER CONSTRUCTION BARRIER SET TEMPORARILY IN PLACE TO SHIELD VEHICULAR AND PEDESTRIAN TRAFFIC.
- WHERE DICTATED BY LOCAL CONDITIONS, THE CONTRACTOR SHALL MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN AND WORKER CROSSING LOCATIONS IN ACCORDANCE WITH ALL APPLICABLE CODES AND OSHA REQUIREMENTS.
- CONSTRUCTION ZONE SPEED LIMIT IF REDUCED FROM POSTED LIMITS SHALL BE IN ACCORDANCE WITH MUTCD AND WILL BE DETERMINED BY THE AUTHORITY HAVING JURISDICTION.
- THERE SHALL BE NO WORKERS, EQUIPMENT, OR OTHER VEHICLES IN THE BUFFER SPACE OR THE ROLL AHEAD SPACE.
- DRIVEWAYS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN SHALL BE PROVIDED WITH AT LEAST ONE W20-1 SIGN (ROAD WORK AHEAD) AS A MINIMUM.
- CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE AUTHORITY HAVING JURISDICTION PROVIDED THEY COMPLY WITH MUTCD.
- THE SPACING BETWEEN CONES, TUBULAR MARKERS, VERTICAL PANELS, DRUMS, AND BARRICADES SHOULD NOT EXCEED A DISTANCE IN FEET EQUAL TO 1.0 TIMES THE SPEED LIMIT IN MPH WHEN USED FOR TAPER CHANNELIZATION, AND A DISTANCE IN FEET EQUAL TO 2.0 TIMES THE SPEED LIMIT IN MPH WHEN USED FOR TANGENT CHANNELIZATION.
- WHEN CHANNELIZATION DEVICES HAVE THE POTENTIAL OF LEADING VEHICULAR TRAFFIC OUT OF THE INTENDED VEHICULAR TRAFFIC SPACE, THE CHANNELIZATION DEVICES SHOULD BE EXTENDED A DISTANCE IN FEET OF 2.0 TIMES THE SPEED LIMIT IN MPH BEYOND THE DOWNSTREAM END OF THE TRANSITION AREA.
- TAPER LENGTHS ARE CALCULATED AS FOLLOWS:  
 $L = WS^2/60$  (40 MPH AND HIGHER) OR  $L2 = WS$  (OVER 40 MPH),  
 WHERE W= OFFSET WIDTH (FT), S= TRAFFIC SPEED (MPH).



**TYPICAL PEDESTRIAN / WORKER SAFETY PLAN**

SCALE: NOT TO SCALE

1

**mobilitie**  
intelligent infrastructure

**MOBILITIE, LLC**

PROJECT NO: 9CAB009302-A  
 DRAWN BY: DWW  
 CHECKED BY: JO

B	10/13/16	UC INCORPORATED
A	04/28/16	FOR CLIENT REVIEW

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

**PRELIMINARY**

9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
**VEHICULAR TRAFFIC CONTROL PLAN**

SHEET NUMBER  
**TC-2**

**GENERAL CONSTRUCTION NOTES:**

1. ALL WORK SHALL CONFORM TO THE REQUIREMENTS OF THE LOCAL BUILDING CODE, THE LATEST ADOPTED EDITION AND ALL OTHER APPLICABLE CODES AND ORDINANCES.
2. CONTRACTOR SHALL CONSTRUCT SITE IN ACCORDANCE WITH THESE DRAWINGS AND LATEST MOBILITIE CONSTRUCTION STANDARDS. THE SPECIFICATION IS THE RULING DOCUMENT AND ANY DISCREPANCIES BETWEEN THE SPECIFICATION AND THE CONSTRUCTION DRAWINGS SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT/ENGINEER OR MOBILITIE CM PRIOR TO THE COMMENCEMENT OF WORK.
3. CONTRACTOR SHALL VISIT THE JOB SITE AND SHALL FAMILIARIZE THEMSELVES WITH ALL CONDITIONS AFFECTING THE (N) WORK AND SHALL MAKE PROVISIONS AS TO THE COST THEREOF. CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING THEMSELVES WITH ALL CONTRACT DOCUMENTS, FIELD CONDITIONS AND DIMENSIONS AND CONFIRMING THAT THE WORK MAY BE ACCOMPLISHED, AS SHOWN, PRIOR TO PROCEEDING WITH CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT/ENGINEER OR MOBILITIE CM PRIOR TO THE COMMENCEMENT OF WORK. NO COMPENSATION WILL BE AWARDED BASED ON CLAIM OF LACK OF KNOWLEDGE OF FIELD CONDITIONS.
4. IT IS NOT THE INTENT OF THESE PLANS TO SHOW EVERY MINOR DETAIL OF CONSTRUCTION. CONTRACTOR IS REQUIRED TO FURNISH AND INSTALL ANY/ALL ITEMS FOR A COMPLETE AND FULLY FUNCTIONAL SYSTEM SUBJECT ONLY TO OWNER-SUPPLIED ITEMS. CONTRACTOR SHALL PROVIDE ANY/ALL REQUIREMENTS FOR THE EQUIPMENT TO BE PLACED IN PROPER WORKING ORDER.
5. PLANS ARE NOT TO BE SCALED. THESE PLANS ARE INTENDED TO BE A DIAGRAMMATIC OUTLINE ONLY UNLESS OTHERWISE NOTED. THE WORK SHALL INCLUDE FURNISHING MATERIALS, EQUIPMENT AND APPURTENANCES, AND LABOR NECESSARY TO EFFECT ALL INSTALLATIONS AS INDICATED ON THE DRAWINGS. OWNER PROVIDED AND CONTRACTOR INSTALLED MATERIALS WILL INCLUDE THE FOLLOWING, UNLESS NOTED OTHERWISE:
  - A) TRANSMITTER
  - B) UHF ANTENNA AND MOUNTING BRACKETS, GPS ANTENNAS AND KU ANTENNAS
  - C) UHF COAX AND HANGERS
  - D) INTEGRATED LOAD CENTER
6. DIMENSIONS SHOWN ARE TO FINISH SURFACES UNLESS OTHERWISE NOTED. SPACING BETWEEN EQUIPMENT IS REQUIRED CLEARANCE. THEREFORE, IT IS CRITICAL TO FIELD VERIFY DIMENSIONS. SHOULD THERE BE ANY QUESTIONS REGARDING THE CONTRACT DOCUMENTS, (E) CONDITIONS AND/OR DESIGN INTENT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPORTING ANY DISCREPANCIES TO THE ATTENTION OF THE MOBILITIE CM, IN WRITING, PRIOR TO THE COMMENCEMENT OF WORK.
7. DETAILS PROVIDED ARE FOR THE PURPOSE OF SHOWING DESIGN INTENT. MODIFICATIONS MAY BE REQUIRED TO SUIT JOB DIMENSIONS OR SITE CONDITIONS, AND SUCH MODIFICATIONS SHALL BE INCLUDED AS PART OF THE WORK.
8. CONTRACTOR SHALL PAY FOR APPLICABLE PERMITS, FEES, INSPECTIONS AND TESTING. CONTRACTOR IS TO OBTAIN PERMITS AND APPROVED SUBMITTALS PRIOR TO ORDERING MATERIALS AND THE COMMENCEMENT OF WORK.
9. THE TERM "PROVIDE" USED IN CONSTRUCTION DOCUMENTS AND SPECIFICATIONS, INDICATES THAT THE CONTRACTOR SHALL FURNISH AND INSTALL.
10. CONTRACTOR SHALL RECEIVE CLARIFICATION IN WRITING, AND SHALL RECEIVE IN WRITING AUTHORIZATION TO PROCEED BEFORE STARTING WORK ON ANY ITEMS NOT CLEARLY DEFINED OR IDENTIFIED BY THE CONTRACT DOCUMENTS.
11. CONTRACTOR SHALL SUPERVISE AND DIRECT THE WORK USING ACCEPTED INDUSTRY-STANDARD SKILLS AND ATTENTION. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES AND PROCEDURES AND FOR COORDINATING ALL PORTIONS OF THE WORK UNDER CONTRACT, UNLESS OTHERWISE NOTED.
12. CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF THE WORK AREA, ADJACENT AREAS AND BUILDING OCCUPANTS THAT ARE LIKELY TO BE AFFECTED BY THE WORK UNDER THIS CONTRACT. WORK SHALL CONFORM TO ALL OSHA REQUIREMENTS.
13. CONTRACTOR SHALL COORDINATE THEIR WORK WITH THE MOBILITIE CM AND SCHEDULE THEIR ACTIVITIES AND WORKING HOURS IN ACCORDANCE WITH THE REQUIREMENTS.

14. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THEIR WORK WITH THE WORK OF OTHERS AS IT MAY RELATE TO RADIO EQUIPMENT, ANTENNAS AND ANY OTHER PORTIONS OF THE WORK.
15. CONTRACTOR SHALL INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS UNLESS SPECIFICALLY OTHERWISE INDICATED OR WHERE LOCAL CODES OR REGULATIONS TAKE PRECEDENCE.
16. CONTRACTOR SHALL MAKE NECESSARY PROVISIONS TO PROTECT (E) SURFACES, EQUIPMENT, IMPROVEMENTS, PIPING ETC. AND IMMEDIATE REPAIR, TO NEW CONDITION, ANY DAMAGE THAT OCCURS DURING CONSTRUCTION AT THE SOLE COST OF THE CONTRACTOR.
17. IN DRILLING HOLES, OR CORING, INTO CONCRETE WHETHER FOR FASTENING OR ANCHORING PURPOSES, OR PENETRATIONS THROUGH THE FLOOR FOR CONDUIT RUNS, PIPE RUNS, ETC., MUST BE CLEARLY UNDERSTOOD THAT REINFORCING STEEL SHALL NOT BE DRILLED INTO, CUT OR DAMAGED UNDER ANY CIRCUMSTANCES (UNLESS NOTED OTHERWISE). LOCATIONS OF REINFORCING STEEL ARE NOT DEFINITELY KNOWN AND THEREFORE MUST BE LOCATED BY THE CONTRACTOR USING APPROPRIATE METHODS AND EQUIPMENT PRIOR TO ANY DRILLING OR CORING OPERATIONS IN (E) CONCRETE.
18. CONTRACTOR SHALL REPAIR, TO NEW CONDITION, ALL (E) WALL SURFACES DAMAGED DURING CONSTRUCTION SUCH THAT THEY MATCH AND BLEND IN WITH ADJACENT SURFACES.
19. CONTRACTOR SHALL SEAL PENETRATIONS THROUGH FIRE RATED ASSEMBLIES OR MATERIALS WITH U.L. LISTED AND FIRE CODE APPROVED MATERIALS AND SYSTEMS THAT MEET OR EXCEED THE RATING OF THE ASSEMBLY IN WHICH THE NEW PENETRATION IS PLACED.
20. CONTRACTOR SHALL KEEP CONTRACT AREA CLEAN, HAZARD FREE, AND DISPOSE OF ALL DIRT, DEBRIS, AND RUBBISH. EQUIPMENT NOT SPECIFIED AS REMAINING ON THE PROPERTY OF THE OWNER SHALL BE REMOVED. LEAVE PREMISES IN CLEAN CONDITION AND FREE FROM PAINT SPOTS, DUST, OR SMUDGES OF ANY NATURE. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ITEMS UNTIL COMPLETION OF CONSTRUCTION.
21. MINIMUM BEND RADIUS OF ANTENNA CABLES SHALL BE IN ACCORDANCE WITH CABLE MANUFACTURERS RECOMMENDATIONS.
22. CONTRACTOR SHALL MINIMIZE DISTURBANCE TO (E) SITE DURING CONSTRUCTION. EROSION CONTROL MEASURES, IF REQUIRED DURING CONSTRUCTION SHALL BE IN CONFORMANCE WITH JURISDICTIONAL OR STATE AND LOCAL GUIDELINES FOR EROSION AND SEDIMENT CONTROL AND COORDINATED WITH LOCAL REGULATORY AUTHORITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF ANY EROSION CONTROL MEASURES, RECORD-KEEPING, MONITORING, AND REPORTING TO THE OWNER AND REGULATORY AUTHORITIES.
23. ALL CONSTRUCTION WORK IS TO ADHERE TO APPLICANT'S INTEGRATED CONSTRUCTION STANDARDS UNLESS STATE OR LOCAL CODE IS MORE STRINGENT.
24. THE INTENT OF THE PLANS AND SPECIFICATIONS IS TO PERFORM THE CONSTRUCTION IN ACCORDANCE PER STATE BUILDING STANDARDS CODE AND STATE CODE OF REGULATIONS. SHOULD ANY CONDITIONS DEVELOP NOT COVERED BY THE APPROVED PLANS AND SPECIFICATIONS WHEREIN THE FINISHED WORK WILL NOT COMPLY PER STATE CODE OF REGULATIONS, A SCOPE OF WORK DETAILING AND SPECIFYING THE REQUIRED WORK SHALL BE SUBMITTED TO AND APPROVED BY THE JURISDICTION BEFORE PROCEEDING WITH THE WORK. A CHANGE ORDER FOR THAT SCOPE SHALL BE SUBMITTED TO THE MOBILITIE CM PRIOR TO PROCEEDING WITH THE WORK.
25. ADEQUATE AND REQUIRED LIABILITY INSURANCE SHALL BE PROVIDED BY THE CONTRACTOR FOR PROTECTION AGAINST PUBLIC LOSS AND ANY/ALL PROPERTY DAMAGE FOR THE DURATION OF WORK.
26. CONTRACTOR SHALL GUARANTEE ANY/ALL MATERIALS AND WORK FREE FROM DEFECTS FOR A PERIOD OF NOT LESS THAN ONE YEAR FROM DATE OF ACCEPTANCE. ANY CORRECTIVE WORK SHALL BE COMPLETED AT THE SOLE COST OF THE CONTRACTOR.

**ELECTRICAL NOTES:**

1. ELECTRICAL CONTRACTOR SHALL SUPPLY AND INSTALL ANY/ALL ELECTRICAL WORK INDICATED. ANY/ALL CONSTRUCTION SHALL BE IN ACCORDANCE W/DRAWINGS AND ANY/ALL APPLICABLE SPECIFICATIONS. IF ANY PROBLEMS ARE ENCOUNTERED BY COMPLYING WITH THESE REQUIREMENTS, CONTRACTOR SHALL NOTIFY MOBILITIE CM AS SOON AS POSSIBLE, AFTER THE DISCOVERY OF THE PROBLEMS, AND SHALL NOT PROCEED WITH THAT PORTION OF WORK, UNTIL THE MOBILITIE CM HAS DIRECTED THE CORRECTIVE ACTIONS TO BE TAKEN.

2. ELECTRICAL CONTRACTOR SHALL VISIT THE JOB SITE AND FAMILIARIZE THEMSELF WITH ANY/ALL CONDITIONS AFFECTING ELECTRICAL AND COMMUNICATION INSTALLATION AND MAKE PROVISIONS AS TO THE COST THEREOF. ALL (E) CONDITIONS OF ELECTRICAL EQUIP., ETC., THAT ARE PART OF THE FINAL SYSTEM, SHALL BE VERIFIED BY THE CONTRACTOR, PRIOR TO THE SUBMITTING OF THEIR BID. FAILURE TO COMPLY WITH THIS PARAGRAPH WILL IN NO WAY RELIEVE CONTRACTOR OF PERFORMING ALL WORK NECESSARY FOR A COMPLETE AND WORKING SYSTEM.
3. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST EDITION OF THE NEC, ALL CODES AND ORDINANCES OF THE LOCAL JURISDICTION, AND POWER & TELEPHONE COMPANIES HAVING JURISDICTION AND SHALL INCLUDE BUT ARE NOT BE LIMITED TO:
  - A) UL – UNDERWRITERS LABORATORIES
  - B) NEC – NATIONAL ELECTRICAL CODE
  - C) NEMA – NATIONAL ELECTRICAL MANUFACTURERS ASSOC.
  - D) OSHA – OCCUPATIONAL SAFETY AND HEALTH ACT
  - E) SBC – STANDARD BUILDING CODE
  - F) NFPA – NATIONAL FIRE PROTECTION AGENCY
  - G) ANSI – AMERICAN NATIONAL STANDARDS INSTITUTE
  - H) IEEE – INSTITUTE OF ELECTRICAL AND ELECTRONICS ENGINEERS
  - I) ASTM – AMERICAN SOCIETY FOR TESTING MATERIALS
4. REFER TO SITE PLANS AND ELEVATIONS FOR EXACT LOCATIONS OF ALL EQUIPMENT, AND CONFIRM WITH MOBILITIE CM ANY SIZES AND LOCATIONS WHEN NEEDED.
5. (E) SERVICES: CONTRACTOR SHALL NOT INTERRUPT (E) SERVICES WITHOUT WRITTEN PERMISSION OF THE OWNER.
6. CONTRACTOR SHALL CONFIRM WITH LOCAL UTILITY COMPANY ANY/ALL REQUIREMENTS SUCH AS THE: LUG SIZE RESTRICTIONS, CONDUIT ENTRY, SIZE OF TRANSFORMERS, SCHEDULED DOWNTIME FOR THE OWNERS' CONFIRMATION, ETC... ANY/ALL CONFLICTS SHALL BE BROUGHT TO THE ATTENTION OF THE MOBILITIE CM, PRIOR TO BEGINNING ANY WORK.
7. MINIMUM WIRE SIZE SHALL BE #12 AWG, NOT INCLUDING CONTROL WIRING, UNLESS NOTED OTHERWISE. ALL CONDUCTORS SHALL BE COPPER WITH THWN INSULATION, UNLESS OTHERWISE NOTED.
8. OUTLET BOXES SHALL BE PRESSED STEEL IN DRY LOCATIONS, CAST ALLOY WITH THREADED HUBS IN WET/DAMP LOCATIONS AND SPECIAL ENCLOSURES FOR OTHER CLASSIFIED AREAS.
9. IT IS NOT THE INTENT OF THESE PLANS TO SHOW EVERY MINOR DETAIL OF THE CONSTRUCTION. CONTRACTOR IS EXPECTED TO FURNISH AND INSTALL ALL ITEMS FOR A COMPLETE ELECTRICAL SYSTEM AND PROVIDE ALL REQUIREMENTS FOR THE EQUIPMENT TO BE PLACED IN PROPER WORKING ORDER.
10. ELECTRICAL SYSTEM SHALL BE AS COMPLETELY AND EFFECTIVELY GROUNDED, AS REQUIRED BY SPECIFICATIONS, SET FORTH BY APPLICANT.
11. ALL WORK SHALL BE PERFORMED BY A LICENSED ELECTRICAL CONTRACTOR IN A FIRST CLASS, WORKMANLIKE MANNER. THE COMPLETED SYSTEM SHALL BE FULLY FUNCTIONAL AND SHALL BE APPROVED BY THE MOBILITIE CM AND LOCAL JURISDICTION. ANY DEFICIENCIES SHALL BE CORRECTED BY AN ELECTRICAL CONTRACTOR AT THE SOLE COST OF THE CONTRACTOR.
12. ALL WORK SHALL BE COORDINATED WITH OTHER TRADES TO AVOID INTERFERENCE WITH THE PROGRESS OF CONSTRUCTION.



**MOBILITIE, LLC**

PROJECT NO: 9CAB009302-A  
 DRAWN BY: DWW  
 CHECKED BY: JO

B	10/13/16	UC INCORPORATED
A	04/28/16	FOR CLIENT REVIEW

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**PRELIMINARY**

9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
**GENERAL NOTES**

SHEET NUMBER  
**GN-1**

ELECTRICAL NOTES CONT'D

13. THE CORRECTION OF ANY DEFECTS SHALL BE COMPLETED BY THE CONTRACTOR WITHOUT ANY ADDITIONAL CHARGE AND SHALL INCLUDE THE REPLACEMENT OR THE REPAIR OF ANY OTHER PHASE OF THE INSTALLATION, WHICH MAY HAVE BEEN DAMAGED THEREIN.
14. CONTRACTOR SHALL PROVIDE AND INSTALL CONDUIT, CONDUCTORS, PULL WIRES, BOXES, COVER PLATES AND DEVICES FOR ALL OUTLETS AS INDICATED.
15. DITCHING AND BACK FILL: CONTRACTOR SHALL PROVIDE FOR ALL UNDERGROUND INSTALLED CONDUIT AND/OR CABLES INCLUDING EXCAVATION AND BACKFILLING AND COMPACTION. REFER TO NOTES AND REQUIREMENTS 'EXCAVATION, AND BACKFILLING.
16. MATERIALS, PRODUCTS AND EQUIPMENT, INCLUDING ALL COMPONENTS THEREOF, SHALL BE NEW AND SHALL APPEAR ON THE LIST OF U.L. APPROVED ITEMS AND SHALL MEET OR EXCEED THE REQUIREMENTS OF THE NEC, NEMA AND IEC.
17. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR MANUFACTURER'S CATALOG INFORMATION OF ANY/ALL EQUIPMENT AND ALL OTHER ELECTRICAL ITEMS FOR APPROVAL BY THE MOBILITIE CM PRIOR TO INSTALLATION.
18. ANY CUTTING OR PATCHING DEEMED NECESSARY FOR ELECTRICAL WORK IS THE ELECTRICAL CONTRACTORS RESPONSIBILITY AND SHALL BE INCLUDED IN THE COST FOR WORK AND PERFORMED TO THE SATISFACTION OF THE MOBILITIE CM UPON FINAL ACCEPTANCE.
19. THE ELECTRICAL CONTRACTOR SHALL LABEL ALL PANELS WITH ONLY TYPEWRITTEN DIRECTORIES. ALL ELECTRICAL WIRING SHALL BE THE RESPONSIBILITY OF THE ELECTRICAL CONTRACTOR.
20. DISCONNECT SWITCHES SHALL BE UL-RATED, H.P. RATED HEAVY-DUTY, QUICK-MAKE AND QUICK-BREAK ENCLOSURES, AS REQUIRED BY EXPOSURE TYPE.
21. ALL CONNECTIONS SHALL BE MADE WITH A PROTECTIVE COATING OF AN ANTI-OXIDE COMPOUND KNOWN AS "NO-OXIDE A" BY DEARBORNE CHEMICAL CO. COAT ALL WIRE SURFACES BEFORE CONNECTING. EXPOSED COPPER SURFACES, INCLUDING GROUND BARS, SHALL BE TREATED - NO SUBSTITUTIONS.
22. RACEWAYS: CONDUIT SHALL BE SCHEDULE 80 PVC MEETING OR EXCEEDING NEMA TC2 - 1990. CONTRACTOR SHALL PLUG AND CAP EACH END OF SPARE AND EMPTY CONDUITS AND PROVIDE TWO SEPARATE PULL STRINGS - 200 LBS TEST POLYETHYLENE CORD. ALL CONDUIT BENDS SHALL BE A MINIMUM OF 2 FT. RADIUS. RGS CONDUITS WHEN SPECIFIED, SHALL MEET UL-6 FOR GALVANIZED STEEL. ALL FITTINGS SHALL BE SUITABLE FOR USE WITH THREADED RIGID CONDUIT. COAT ALL THREADS WITH 'BRITZ ZINC' OR 'COLD GALV'.
23. SUPPORT OF ALL ELECTRICAL WORK SHALL BE AS REQUIRED BY NEC.
24. CONDUCTORS: CONTRACTOR SHALL USE 98% CONDUCTIVITY COPPER WITH TYPE THWN INSULATION, UNLESS OTHERWISE NOTED, 600 VOLT, COLOR CODED. USE SOLID CONDUCTORS FOR WIRE UP TO AND INCLUDING NO. 8 AWG. USE STRANDED CONDUCTORS FOR WIRE ABOVE NO. 8 AWG.
25. CONNECTORS FOR POWER CONDUCTORS: CONTRACTOR SHALL USE PRESSURE TYPE INSULATED TWIST-ON CONNECTORS FOR NO. 10 AWG AND SMALLER. USE SOLDERLESS MECHANICAL TERMINAL LUGS FOR NO. 8 AWG AND LARGER.
26. SERVICE: AS SPECIFIED ON THE DRAWINGS. OWNER OR OWNER'S AGENT WILL APPLY FOR POWER. ALL PROVISIONS FOR TEMPORARY POWER WILL BE OBTAINED BY THE CONTRACTOR.
27. TELEPHONE OR FIBER SERVICE: CONTRACTOR SHALL PROVIDE EMPTY CONDUITS WITH PULL STRINGS AS INDICATED ON DRAWINGS.
28. ELECTRICAL AND TELCO/FIBER RACEWAYS TO BE BURIED A MINIMUM DEPTH OF 30", UNLESS OTHERWISE NOTED.
29. CONTRACTOR SHALL PLACE 6" WIDE DETECTABLE WARNING TAPE AT A DEPTH OF 6" BELOW GROUND AND DIRECTLY ABOVE ELECTRICAL AND TELCO SERVICE CONDUITS. CAUTIONS TAPE TO READ "CAUTION BURIED ELECTRIC" OR "BURIED TELECOMM".
30. ALL BOLTS SHALL BE 3-16 STAINLESS STEEL

GROUNDING NOTES:

1. ALL HARDWARE SHALL BE 3-16 STAINLESS STEEL, INCLUDING LOCK WASHERS. COAT ALL SURFACES WITH AN ANTI-OXIDANT COMPOUND, AS SPECIFIED, BEFORE MATING. ALL HARDWARE SHALL BE STAINLESS STEEL 3/8 INCH DIAMETER OR LARGER.
2. FOR GROUND BOND TO STEEL ONLY: INSERT A CADMIUM FLAT WASHER BETWEEN LUG AND STEEL, COAT ALL SURFACES WITH AN ANTI-OXIDANT COMPOUND BEFORE MATING.
3. ALL STEEL CONDUIT SHALL BE BONDED AT BOTH ENDS WITH GROUNDING BUSHING.
4. ALL ELECTRICAL AND GROUNDING AT THE POLE SITE SHALL COMPLY WITH THE NATIONAL ELECTRICAL CODE (NEC), NATIONAL FIRE PROTECTION ASSOCIATION (NFPA) 780 (LATEST EDITION), AND MANUFACTURER.
5. ALL DETAILS ARE SHOWN IN GENERAL TERMS. ACTUAL GROUNDING INSTALLATION AND CONSTRUCTION MAY VARY DUE TO SITE SPECIFIC CONDITIONS.
6. GROUND ALL ANTENNA BASES, FRAMES, CABLE RUNS, AND OTHER METALLIC COMPONENTS USING #6 GROUND WIRES. FOLLOW ANTENNA AND BTS MANUFACTURER'S PRACTICES FOR GROUNDING REQUIREMENTS.
7. ALL GROUND CONNECTIONS SHALL BE #6 AWG, UNLESS OTHERWISE NOTED. ALL WIRES SHALL BE COPPER WITH THHN, UNLESS OTHERWISE NOTED. ALL GROUND WIRE SHALL BE SOLID TIN COATED OR STRANDED GREEN INSULATED WIRE.
8. CONTRACTOR TO VERIFY AND TEST GROUND TO SOURCE, 10 OHMS MAXIMUM. PROVIDE SUPPLEMENT GROUNDING RODS AS REQUIRED TO ACHIEVE SPECIFIED OHMS READING. GROUNDING AND OTHER OPTIONAL TESTING WILL BE WITNESSED BY THE MOBILITIE CM.
9. NOTIFY ARCHITECT/ENGINEER IF THERE ARE ANY DIFFICULTIES INSTALLING GROUNDING SYSTEM DUE TO SITE SOIL CONDITIONS.
10. ALL HORIZONTALLY RUN GROUNDING CONDUCTORS SHALL BE INSTALLED A MINIMUM OF 30" BELOW GRADE/ 6" BELOW FROST-LINE IN TRENCH, UNLESS OTHERWISE NOTED. BACK FILL SHALL BE COMPACTED AS REQUIRED BY ARCHITECT/ENGINEER.
11. ALL GROUND CONDUCTORS SHALL BE RUN AS STRAIGHT AND SHORT AS POSSIBLE, WITH A MINIMUM 12" BENDING RADIUS NOT LESS THAN 90 DEGREES.
12. ACCEPTABLE CONNECTIONS FOR GROUNDING SYSTEM SHALL BE:
  - A. BURNDY, HY-GRADE U.L. LISTED CONNECTORS FOR OUTDOOR USE OR AS APPROVED BY APPLICANT PROJECT MANAGER.
  - B. CADWELD, EXOTHERMIC WELDS (WELDED CONNECTIONS).
  - C. ONE (1) HOLE TINNED COPPER COMPRESSION (LONG BARREL) FITTINGS.
13. ALL CRIMPED CONNECTIONS SHALL HAVE EMBOSSED MANUFACTURER'S DIEMARK VISIBLE AT THE CRIMP (RESULTING FROM USE OF PROPER CRIMPING DEVICES) AND WEATHER-PROOFED WITH HEAT SHRINK.
14. ALL CONNECTION HARDWARE SHALL BE TYPE 3-16 STAINLESS STEEL (NOT ATTRACTED TO MAGNETS).
15. ELECTRICAL SERVICE EQUIPMENT GROUNDING SHALL COMPLY WITH NEC, ARTICLE 250-82 AND SHALL BOND ALL (E) AND NEW GROUNDING ELECTRODES. NEW GROUNDING ELECTRODE SHALL INCLUDE BUT NOT LIMITED TO GROUND RODS.

TESTING AND EQUIPMENT TURN UP REQUIREMENTS:

1. RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT TESTING WILL COMPLY WITH CURRENT INDUSTRY STANDARDS AND OR THOSE STANDARDS OF THE EQUIPMENT MANUFACTURER OR PROVIDED TO THE CONTRACTOR PRIOR TO TESTING.
2. CONTRACTOR WILL USE THE APPROPRIATE CALIBRATED TESTING EQUIPMENT IN THE TESTING OF RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT THAT MEET INDUSTRY STANDARDS OF THE MANUFACTURER OR THOSE STANDARDS PROVIDED TO THE CONTRACTOR PRIOR TO TESTING.

3. CONTRACTOR TO VERIFY AND RECORD ALL TEST RESULTS AND PROVIDE THESE RESULTS WITHIN THE FINAL CLOSE OUT PACKAGE.
4. ALL PERSONNEL INVOLVED IN THE TESTING OF RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT WILL BE REQUIRED TO HAVE BEEN TRAINED AND OR CERTIFIED IN THE PROPER TESTING OF RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT.
5. ALL TEST RESULTS SHALL BE TIME STAMPED, RECORDED AND PRESENTED PRIOR TO ENERGIZING AND TURN UP OF ANY EQUIPMENT.
6. GPS EQUIPMENT IS NOT TO BE TESTED OR ATTACHED TO ANY CABLING DURING TESTING, DOING SO WILL DAMAGE THE GPS UNIT.
7. PRIOR TO TESTING IF THE CONTRACTOR HAS ANY QUESTIONS ABOUT THE TESTING PROCEDURES THEY ARE TO CALL AND OBTAIN ASSISTANCE FROM A QUALIFIED DESIGNATED TESTING REPRESENTATIVE.
8. EQUIPMENT IS NOT TO BE ENERGIZED UNTIL ALL TESTING HAS BEEN COMPLETED, APPROVED AND THE APPROPRIATE AUTHORITY HAS BEEN NOTIFIED AND GIVES APPROVAL TO ENERGIZE THE EQUIPMENT.

SITE WORK NOTES:

1. DO NOT EXCAVATE OR DISTURB BEYOND THE PROPERTY LINES OR LEASE LINES, UNLESS OTHERWISE NOTED.
2. SIZE, LOCATION AND TYPE OF ANY UNDERGROUND UTILITIES OR IMPROVEMENTS SHALL BE ACCURATELY NOTED AND PLACED ON AS-BUILT DRAWINGS BY GENERAL CONTRACTOR AND ISSUED TO ARCHITECT/ENGINEER AT COMPLETION OF PROJECT.
3. ALL (E) UTILITIES, FACILITIES, CONDITIONS AND THEIR DIMENSIONS SHOWN ON PLANS HAVE BEEN PLOTTED FROM AVAILABLE RECORDS. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER AS TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS OR THE MANNER OF THEIR REMOVAL OR ADJUSTMENT. CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING EXACT LOCATION OF ALL (E) UTILITIES AND FACILITIES PRIOR TO START OF CONSTRUCTION. CONTRACTOR SHALL ALSO OBTAIN FROM EACH UTILITY COMPANY DETAILED INFORMATION RELATIVE TO WORKING SCHEDULES AND METHODS OF REMOVING OR ADJUSTING (E) UTILITIES.
4. CONTRACTOR SHALL VERIFY ALL (E) UTILITIES BOTH HORIZONTALLY AND VERTICALLY PRIOR TO START OF CONSTRUCTION. ANY DISCREPANCIES OR DOUBTS AS TO THE INTERPRETATION OF PLANS SHALL BE IMMEDIATELY REPORTED TO THE ARCHITECT/ENGINEER OR MOBILITIE CM FOR RESOLUTION AND INSTRUCTION, AND NO FURTHER WORK SHALL BE PERFORMED UNTIL THE DISCREPANCY IS CHECKED AND CORRECTED BY THE ARCHITECT/ENGINEER. FAILURE TO SECURE SUCH INSTRUCTION MEANS CONTRACTOR WILL HAVE WORKED AT THEIR OWN RISK AND EXPENSE. CONTRACTOR SHALL CALL LOCAL UTILITY LOCATE HOT LINE, SUCH AS 811, FOR UTILITY LOCATIONS A MINIMUM OF 48 HOURS PRIOR TO START OF CONSTRUCTION.
5. ALL NEW AND (E) UTILITY STRUCTURES ON SITE AND IN AREAS TO BE DISTURBED BY CONSTRUCTION SHALL BE ADJUSTED TO FINISH ELEVATIONS PRIOR TO FINAL INSPECTION OF WORK. ANY COST RELATED TO ADJUSTING (E) STRUCTURES SHALL BE BORNE SOLELY BY THE CONTRACTOR.
6. GRADING OF THE SITE WORK AREA IS TO BE SMOOTH AND CONTINUOUS IN SLOPE AND IS TO FEATHER INTO (E) GRADES AT THE GRADING LIMITS.
7. ALL TEMPORARY EXCAVATIONS FOR THE INSTALLATION OF FOUNDATIONS, UTILITIES, ETC., SHALL BE PROPERLY LAID BACK OR BRACED IN ACCORDANCE WITH CORRECT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REQUIREMENTS.



**MOBILITIE, LLC**

PROJECT NO:	9CAB009302-A
DRAWN BY:	DWW
CHECKED BY:	JO

B	10/13/16	UC INCORPORATED
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PRELIMINARY

9CAB009302-A  
RIVERBANK CITY, CA 95367  
EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
GENERAL NOTES

SHEET NUMBER  
GN-2

SITE WORK NOTES CONT'D

8. STRUCTURAL FILLS SUPPORTING PAVEMENTS SHALL BE COMPACTED TO 95% OF MAXIMUM STANDARD PROCTOR DRY DENSITY, UNLESS OTHERWISE NOTED.
9. NEW GRADES NOT IN BUILDING AND DRIVEWAY IMPROVEMENT AREA TO BE ACHIEVED BY FILLING WITH APPROVED CLEAN FILL AND COMPACTED TO 95% OF STANDARD PROCTOR DENSITY.
10. ALL FILL SHALL BE PLACED IN UNIFORM LIFTS. THE LIFTS THICKNESS SHOULD NOT EXCEED THAT WHICH CAN BE PROPERLY COMPACTED THROUGHOUT ITS ENTIRE DEPTH WITH THE EQUIPMENT AVAILABLE.
11. ANY FILLS PLACED ON (E) SLOPES THAT ARE STEEPER THAN 10 HORIZONTAL TO 1 VERTICAL SHALL BE PROPERLY BENCHED INTO THE (E) SLOPE AS DIRECTED BY A GEOTECHNICAL ENGINEER.
12. CONTRACTOR SHALL CLEAN ENTIRE SITE AFTER CONSTRUCTION SUCH THAT NO DEBRIS, PAPER, TRASH, WEEDS, BRUSH, EXCESS FILL, OR ANY OTHER DEPOSITS WILL REMAIN. ALL MATERIALS COLLECTED DURING CLEANING OPERATIONS SHALL BE DISPOSED OF OFF-SITE BY THE GENERAL CONTRACTOR.
13. ALL TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONFLICT WITH THE IMPROVEMENTS SHALL BE PROTECTED BY THE GENERAL CONTRACTOR.
14. ALL SITE WORK SHALL BE CAREFULLY COORDINATED BY GENERAL CONTRACTOR WITH LOCAL UTILITY COMPANY, TELEPHONE COMPANY, AND ANY OTHER UTILITY COMPANIES HAVING JURISDICTION OVER THIS LOCATION.

ENVIRONMENTAL NOTES:

1. ALL WORK PERFORMED SHALL BE DONE IN ACCORDANCE WITH ISSUED PERMITS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF FINES AND PROPER CLEAN UP FOR AREAS IN VIOLATION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTION AND MAINTENANCE OF EROSION AND SEDIMENTATION CONTROLS DURING CONSTRUCTION FOR PROTECTION OF ADJACENT PROPERTIES, ROADWAYS AND WATERWAYS. ALL EROSION AND SEDIMENTATION CONTROLS SHALL BE MAINTAINED IN PLACE THROUGH FINAL JURISDICTIONAL INSPECTION & RELEASE OF SITE.
3. CONTRACTOR SHALL INSTALL/CONSTRUCT ALL NECESSARY SEDIMENT/SILT CONTROL FENCING AND PROTECTIVE MEASURES AS REQUIRED BY THE LOCAL JURISDICTION WITHIN THE LIMITS OF SITE DISTURBANCE PRIOR TO CONSTRUCTION.
4. NO SEDIMENT SHALL BE ALLOWED TO EXIT THE PROPERTY. THE CONTRACTOR IS RESPONSIBLE FOR TAKING ADEQUATE MEASURES FOR CONTROLLING EROSION. ADDITIONAL SEDIMENT CONTROL FENCING MAY BE REQUIRED IN ANY AREAS SUBJECT TO EROSION.
5. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING POSITIVE DRAINAGE ON THE SITE AT ALL TIMES WITH SILT AND EROSION CONTROL MEASURES MAINTAINED ON THE DOWNSTREAM SIDE OF SITE DRAINAGE. ANY DAMAGE TO ADJACENT PROPERTY AS A RESULT OF EROSION WILL BE CORRECTED AT THE CONTRACTORS EXPENSE.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR DAILY INSPECTIONS AND ANY REPAIRS OF ALL SEDIMENT CONTROL MEASURES INCLUDING SEDIMENT REMOVAL AS NECESSARY.
7. CLEARING OF VEGETATION AND TREE REMOVAL SHALL BE ONLY AS PERMITTED AND BE HELD TO A MINIMUM. ONLY TREES NECESSARY FOR CONSTRUCTION OF THE FACILITIES SHALL BE REMOVED.
8. SEEDING AND MULCHING AND/OR SODDING OF THE SITE WILL BE ACCOMPLISHED AS SOON AS POSSIBLE AFTER COMPLETION OF THE PROJECT FACILITIES AFFECTING LAND DISTURBANCE.
9. CONTRACTOR SHALL PROVIDE ALL EROSION AND SEDIMENTATION CONTROL MEASURES AS REQUIRED BY LOCAL, COUNTY AND STATE CODES AND ORDINANCES TO PROTECT EMBANKMENTS FROM SOIL LOSS AND TO PREVENT ACCUMULATION OF SOIL AND SILT IN STREAMS AND DRAINAGE PATHS LEAVING THE CONSTRUCTION AREA. THIS MAY INCLUDE, BUT IS NOT LIMITED TO SUCH MEASURES AS SILT FENCES, STRAW BALE SEDIMENT BARRIERS, AND CHECK DAMS.
10. RIP RAP OF SIZES INDICATED SHALL CONSIST OF CLEAN, HARD, SOUND, DURABLE, UNIFORM IN QUALITY STONE FREE OF ANY DETRIMENTAL QUANTITY OF SOFT, FRIABLE, THIN, ELONGATED OR LAMINATED PIECES, DISINTEGRATED MATERIAL, ORGANIC MATTER, OIL, ALKALI, OR OTHER DELETERIOUS SUBSTANCES.

11. GC TO PLACE FILTER MATERIAL AT ALL CATCH BASINS ADJACENT TO CONSTRUCTION SITE TO PREVENT SOLID WASTE CONTAMINATION FROM ENTERING SEWER SYSTEM

FOUNDATION, EXCAVATION AND BACKFILL NOTES:

1. ALL FINAL GRADED SLOPES SHALL BE A MAXIMUM OF 3 HORIZONTAL TO 1 VERTICAL, UNLESS OTHERWISE NOTED.
2. BACKFILL OF POLE SHALL BE PERFORMED IN ONE OF THREE OPTIONS:
  - A. PREFERRED: RAINBOW INDUSTRIES POLE SETTING FOAM SHALL BE INSTALLED PER MANUFACTURER SPECS. FOAM SHALL ALWAYS BE USED FOR POOR SOILS.
  - B. SECONDARY: CONCRETE (REQUIRES MOBILITIE CM WRITTEN APPROVAL) ALLOWABLE SOIL PRESSURE = 2000 PSF (ASSUMED). NON-NATIVE SOILS SHALL BE REMOVE FROM BORE AREA AND SHALL NOT BE REUSED FOR BACKFILL
9. ALL EXCAVATIONS PREPARED FOR PLACEMENT OF CONCRETE SHALL BE OF UNDISTURBED SOILS, SUBSTANTIALLY HORIZONTAL AND FREE FROM ANY LOOSE, UNSUITABLE MATERIAL OR FROZEN SOILS, AND WITHOUT THE PRESENCE OF POUNDING WATER. DEWATERING FOR EXCESS GROUND WATER SHALL BE PROVIDED WHEN REQUIRED. COMPACTION OF SOILS UNDER CONCRETE PAD FOUNDATIONS SHALL NOT BE LESS THAN 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY FOR THE SOIL IN ACCORDANCE WITH ASTM D1557.
10. CONCRETE FOUNDATIONS SHALL NOT BE PLACED ON ORGANIC OR UNSUITABLE MATERIAL. IF ADEQUATE BEARING CAPACITY IS NOT ACHIEVED AT THE DESIGNED EXCAVATION DEPTH, THE UNSATISFACTORY SOIL SHALL BE EXCAVATED TO ITS FULL DEPTH AND EITHER BE REPLACED WITH MECHANICALLY COMPACTED GRANULAR MATERIAL OR THE EXCAVATION SHALL BE FILLED WITH CONCRETE OF THE SAME TYPE SPECIFIED FOR THE FOUNDATION. CRUSHED STONE MAY BE USED TO STABILIZE THE BOTTOM OF THE EXCAVATION. ANY STONE SUB BASE MATERIAL, IF USED, SHALL NOT SUBSTITUTE FOR REQUIRED THICKNESS OF CONCRETE.
11. ALL EXCAVATIONS SHALL BE CLEAN OF UNSUITABLE MATERIAL SUCH AS VEGETATION, TRASH, DEBRIS, AND SO FORTH PRIOR TO BACK FILLING. BACK FILL SHALL CONSIST OF APPROVED MATERIALS SUCH AS EARTH, LOAM, SANDY CLAY, SAND AND GRAVEL, OR SOFT SHALE, FREE FROM CLODS OR LARGE STONES OVER 2 1/2" MAX DIMENSIONS. ALL BACK FILL SHALL BE PLACED IN COMPACTED LAYERS.
12. ALL FILL MATERIALS AND FOUNDATION BACK FILL SHALL BE PLACED IN MAXIMUM 6" THICK LIFTS BEFORE COMPACTION. EACH LIFT SHALL BE WETTED IF REQUIRED AND COMPACTED TO NOT LESS THAN 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY FOR SOIL IN ACCORDANCE WITH ASTM D1557.
13. NEWLY PLACED CONCRETE FOUNDATIONS SHALL CURE A MINIMUM OF 72 HRS PRIOR TO BACK FILLING.
14. FINISHED GRADING SHALL BE SLOPED TO PROVIDE POSITIVE DRAINAGE AND PREVENT STANDING WATER. THE FINAL (FINISH) ELEVATION OF SLAB FOUNDATIONS SHALL SLOPE AWAY IN ALL DIRECTIONS FROM THE CENTER. FINISH GRADE OF CONCRETE PADS SHALL BE A MAXIMUM OF 4 INCHES ABOVE FINAL FINISH GRADE ELEVATIONS. PROVIDE SURFACE FILL GRAVEL TO ESTABLISH SPECIFIED ELEVATIONS WHERE REQUIRED.
15. NEWLY GRADED GRAVEL SURFACE AREAS TO RECEIVE GRAVEL SHALL BE COVERED WITH GEOTEXTILE FABRIC TYPE: TYPAR-3401 AS MANUFACTURED BY TYPAR GEOSYNTHETICS OR AN APPROVED EQUIVALENT, SHOWN ON PLANS. THE GEOTEXTILE FABRIC SHALL BE BLACK IN COLOR TO CONTROL THE RECURRENCE OF VEGETATIVE GROWTH AND EXTEND TO WITHIN 1 FOOT OUTSIDE THE SITE FENCING OR ELECTRICAL GROUNDING SYSTEM PERIMETER WHICH EVER IS GREATER. ALL FABRIC SHALL BE COVERED WITH A MINIMUM OF 4" DEEP COMPACTED STONE OR GRAVEL AS SPECIFIED. I.E. FDOT TYPE NO. 57 FOR FENCED COMPOUND; FDOT TYPE NO. 67 FOR ACCESS DRIVE AREA, UNLESS OTHERWISE NOTED.
16. IN ALL AREAS TO RECEIVE FILL: REMOVE ALL VEGETATION, TOPSOIL, DEBRIS, WET AND UNSATISFACTORY SOIL MATERIALS, OBSTRUCTIONS, AND DELETERIOUS MATERIALS FROM GROUND SURFACE. PLOW STRIP OR BREAK UP SLOPED SURFACES STEEPER THAN 1 VERTICAL TO 4 HORIZONTAL SUCH THAT FILL MATERIAL WILL BIND WITH (E)/PREPARED SOIL SURFACE.
17. WHEN SUB GRADE OR PREPARED GROUND SURFACE HAS A DENSITY LESS THAN THAT REQUIRED FOR THE FILL MATERIAL, SCARIFY THE GROUND SURFACE TO DEPTH REQUIRED, PULVERIZE, MOISTURE-CONDITION AND/OR AERATE THE SOILS AND RECOMPACT TO THE REQUIRED DENSITY PRIOR TO PLACEMENT OF FILLS.
18. IN AREAS WHICH (E) GRAVEL SURFACING IS REMOVED OR DISTURBED DURING CONSTRUCTION OPERATIONS, REPLACE GRAVEL SURFACING TO MATCH ADJACENT

GRAVEL SURFACING AND RESTORED TO THE SAME THICKNESS AND COMPACTION AS SPECIFIED. ALL RESTORED GRAVEL SURFACING SHALL BE FREE FROM CORRUGATIONS AND WAVES.

19. (E) GRAVEL SURFACING MAY NOT BE REUSED.
20. GRAVEL SUB SURFACE SHALL BE PREPARED TO REQUIRED COMPACTION AND SUB GRADE ELEVATIONS BEFORE GRAVEL SURFACING IS PLACED AND/OR RESTORED. ANY LOOSE OR DISTURBED MATERIALS SHALL BE THOROUGHLY COMPACTED AND ANY DEPRESSIONS IN THE SUB GRADE SHALL BE FILLED AND COMPACTED WITH APPROVED SELECTED MATERIAL. GRAVEL SURFACING MATERIAL SHALL NOT BE USED FOR FILLING DEPRESSIONS IN THE SUB GRADE.
21. PROTECT (E) GRAVEL SURFACING AND SUB GRADE IN AREAS WHERE EQUIPMENT LOADS WILL OPERATE. USE PLANKING 'MATTS' OR OTHER SUITABLE PROTECTION DESIGNED TO SPREAD EQUIPMENT LOADS AS MAY BE NECESSARY. REPAIR ANY DAMAGE TO (E) GRAVEL SURFACING OR SUB GRADE WHERE SUCH DAMAGE IS DUE TO THE CONTRACTORS OPERATIONS.
22. DAMAGE TO (E) STRUCTURES AND/OR UTILITIES RESULTING FROM CONTRACTORS NEGLIGENCE SHALL BE REPAIRED AND/ OR REPLACED TO THE OWNERS SATISFACTION AT NO ADDITIONAL COST TO THE CONTRACT.
23. ALL SUITABLE BORROW MATERIAL FOR BACK FILL OF THE SITE SHALL BE INCLUDED IN THE BID. EXCESS TOPSOIL AND UNSUITABLE MATERIAL SHALL BE DISPOSED OF OFF SITE AT LOCATIONS APPROVED BY GOVERNING AGENCIES AT NO ADDITIONAL COST TO THE CONTRACT.



**MOBILITIE, LLC**

PROJECT NO: 9CAB009302-A  
 DRAWN BY: DWW  
 CHECKED BY: JO

B	10/13/16	UC INCORPORATED
A	04/28/16	FOR CLIENT REVIEW

**RECEIVED**  
 DEC 09 2016  
 BY: .....

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

**PRELIMINARY**

9CAB009302-A  
 RIVERBANK CITY, CA 95367  
 EXISTING 26'-0" STEEL LIGHT POLE

SHEET TITLE  
**GENERAL NOTES**

SHEET NUMBER  
**GN-3**

view from Oakdale Road looking north at site



9CAB009302A  
Oakdale Road & CA 108, Riverbank City, CA  
Photosims Produced on 11-30-2016

Existing



Proposed



Existing

**mobilitie**  
intelligent infrastructure

9CAB009302A  
Oakdale Road & CA 108, Riverbank City, CA  
Photosims Produced on 11-30-2016

Proposed



*Existing*



*Proposed*



view from Oakdale Road looking north at site

*Existing*



*Proposed*



Proposed Mobilitie Installation

view from Oakdale Road looking south at site

**CITY OF RIVERBANK  
PLANNING COMMISSION**

**STAFF REPORT**

**ITEM NO:** 3.2 **January 17, 2017**

**APPLICATION:** **Architecture & Site Plan Review 16-0016, and Conditional Use Permit 16-0017 – Del Rio Banquet Hall – 3300 Atchison Street, APN: 132-009-001** - Request for an Architectural and Site Plan Review, proposing a Tenant Improvement for a Banquet Hall, Piano Bar and Grille, and Coffee Shop with upgrades to the façade and the creation of a 456 square foot addition for offices and an outdoor dining area of 196 square foot. In addition, a Conditional Use Permit to serve alcohol Type 47 On-Sale General Eating Establishment is requested. The proposed Architecture and Site Plan review and Conditional Use Permit are subject to approval by the Planning Commission pursuant to Section 153.095(G) of the City of Riverbank Code of Ordinances.

**OWNER:** Tony Zaia

**APPLICANT REPRESENTATIVE:** Jason Huckabee with P & F Builders

**GENERAL PLAN:** Mixed Use (MU)

**ZONING:** Community Commercial (C-2)

**DOWNTOWN SPECIFIC PLAN:** Downtown Core (DC)

**ENVIRONMENTAL DETERMINATION:** The proposed project will not have a significant effect on the environment and is categorically exempt under Article 19 Section 15332 (a-e) Class 32, In-Fill Development Projects.

**PROJECT PLANNER:** Donna M. Kenney, Planning and Building Manager

**RECOMMENDATION:** Approve the Architecture and Site Plan Review, and Conditional Use Permit subject to the conditions in the attached resolutions.

**ACROMYMS:** CEQA – California Environmental Quality Act

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## **I. EXECUTIVE SUMMARY:**

The applicant proposes a tenant improvement for a banquet hall, drive-thru coffee house, piano bar and grille, and a 456 square foot addition for offices and an outdoor dining area of 196 square feet with upgrades to the façade. The applicant is also applying for a Conditional Use Permit in order to sell alcohol at the proposed location. The proposed project will contribute to the enhancement and intensification of the built environment downtown by upgrading and occupying an existing vacant building and empty parking lot. Staff is recommending approval of the application and conditions of approval.

## **II. BACKGROUND & PROJECT INFORMATION:**

The subject property – 3300 Atchison Street – is located at the corner of Atchison Street and Third Street. The site is an existing vacant building that was once a vibrant theatre, restaurant, and banquet hall. The site is surrounded by mixed zoning with Community Commercial and Residential Land Uses surrounding the site.

### **ANALYSIS:**

#### **A. Site Design**

The proposed site plan (Exhibit A) includes tenant improvements and 652 square foot addition to an existing 12,688 square foot building, comprised of a 196 square foot new outdoor area and a 456 square foot addition for office and storage. Trash pick-up will occur behind the building in the existing parking lot. Trash containers will be kept within a covered trash enclosure prior to pick up.

The Applicant is requesting a drive-thru area for the coffee house that is proposed to be constructed on the east side of the existing building. This drive through will be accessible through the alleyway to the north of the site and will feature a right turn only onto adjacent Atchison Street.

## **B. Building Design**

Like most older buildings, building systems need to be updated to comply with current building safety requirements and current codes. The standard condition of approval is that the building will be brought into compliance with current building codes. Any identified structural problems encountered will be addressed as part of the routine implementation of the building permit process.

The Early 20<sup>th</sup> Century Commercial building will undergo both structural and architectural improvements. The building features a well-defined entrance which faces the corner of Atchison and Third Street. The building design is original to the year it was built - 1947. Based on the illustrative drawings presented by the applicant and included as Attachment 2, the building will be repainted various earth tones to enhance the architectural appeal of the building.

The entrance to the building is proposed to be upgraded to enhance curb appeal and to create a vibrant entrance. All windows, doors, and canopies will be upgraded as well as the existing windowpane details.

## **C. Landscaping**

The project's landscaping is limited as most of the site has been developed. Planters and other landscape elements will be added to the site. The Applicant will need to submit official landscape plans by a design professional to the Planning and Building Department for City Staff review prior to building permit issuance, per City of Riverbank standards.

## **D. Transportation and Parking**

According to the Downtown Specific Plan Section 2.3.3, parking requirements for use changes apply only to the incremental increase in parking demand that accompanies a higher use intensity. Although currently vacant, the use was previously a restaurant and banquet hall. The total amount of parking spaces that are required for each use that is being proposed by the applicant is 150 spaces. The property currently has 68 parking spaces on-site with the remainder spaces fulfilled through nearby parking areas, consistent with the Downtown Specific Plan which states, "Non-residential parking requirements may be provided on-site, or off-site within 800 feet of the project site which is within two City blocks."

The Applicant is proposing a drive-thru window that will be accessed through the alley way to the North. The drive-thru will be a one-way aisle and will be feature signage which illustrates a right turn only onto Atchison Street.

## **E. Signage**

The existing 'Del Rio' signage will not be changed with exception of the Zaia family crest, which will be added to the sign. Signage will be added to both sides of the building to enhance curb appeal, and to attract interested customers. The words "CHATEAU DEL RIO", "THE EXCHANGE", and "Coffee House" will be added to the building and will require a separate building permit application per City of Riverbank Standards.

An electronic messaging sign is also proposed for the building, located above the main entrance, which will also require a separate application and City Staff review.

## **III. CONCLUSION:**

The project proposes a permitted use within a vacant building in the downtown. The tenant improvement work and amenities will contribute to the enhancement of the built environment downtown, and hopefully attract other businesses which will serve the same clients.

## **IV. FINDINGS:**

### **A. General Plan Consistency Findings**

Pursuant to the City of Riverbank General Plan, the Planning Commission must make the following findings relevant to the Architectural and Site Plan Review:

- 1. Policy DESIGN-10.1: The City will require site and building access for pedestrians and bicyclists by providing the most direct pedestrian access from sidewalks and parking areas to building entrances while minimizing conflicts with motor vehicle traffic.**

The building entrance opens directly onto city sidewalk with street parking directly beyond it. The applicant is installing a decorative bicycle rack adjacent to the building and will be accommodating ADA accessibility to the site.

- 2. Policy DESIGN-10.8: The City will require ground floor building façade treatments and activities that generate pedestrian interest and comfort. Large windows, canopies, arcades, plazas and outdoor seating are examples of such amenities.**

The architectural style of the building is that of the 1940's and 1950's, and has been vacant for quite some time. Since the building has been vacant upkeep and maintenance to the building has not been conducted leading to the building needing various tenant improvements. The proposed improvements seek to restore the original architecture of the building as well as add some new features which

enhance the character of the site. These improvements and upgrades will generate pedestrian interest in not only the building, but the downtown area as a whole.

**3. Policy DESIGN-10.11: The City will require incorporation of architectural and landscape features that allow for secure locking of bicycles in locations easily observed from indoors.**

The project includes a bicycle rack in an accessible location within 10 feet of the building's front windows.

**B. Conditional Use Permit Findings**

In order to grant any use permit, the applicant must introduce evidence in support of his application sufficient to enable the Planning Commission to find that the establishment, maintenance, or operation of the use or building applied for, under the circumstances of the particular case, be detrimental to the health, safety and general welfare of the persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city. The project is requesting a CUP to serve alcohol in the Piano Bar and Grille and the Banquet Hall. The State will be providing its usual oversight and employees will undergo extensive training before they are allowed to serve alcohol.

**V. ENVIRONMENTAL REVIEW:**

The proposed project will not have a significant effect on the environment and is categorically exempt under Class 32, State CEQA Guidelines 15332 – In-Fill Development Projects, as follows:

Section 15332 exempts projects characterized as in-fill development meeting the conditions described in this section.

**(a) The project is consistent with the application general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.**

The site has a General Plan Designation of Mixed Use (MU), is zoned Community Commercial (C-2), and has a Downtown Specific Plan designation of Downtown Core (DC), which permits the uses that the Applicant has proposed. Further the Applicant has applied for a Conditional Use Permit, which will allow the Applicants to sell alcohol in the piano bar and banquet hall.

**(b) The proposed development occurs within the city limits on a project site of no more than five acres substantially surrounded by urban uses.**

The site is 47,500 square feet in size, and located in the Downtown area of Riverbank which is substantially surrounded by existing public and commercial land uses.

**(c) The project site has no value as habitat for endangered, rare or threatened species.**

The site is previously disturbed in 1947 with no native vegetation currently present on the site.

**(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.**

The project is a tenant improvement within an existing building and paved parking lot. There will be no significant effects to traffic, noise, air quality or water quality. and

**(e) The site can be adequately served by all required utilities and public services.**

All urban utility and public services are present at or adjacent to the site.

**VI. PUBLIC NOTICE:**

The Planning Commission public hearing notice was published in the Riverbank News on January 4, 2017 and posted at City Hall North, South, Post Office, Community Center and website January 4, 2017. In addition, the Applicant posted a Notice of Development Permit Application at 3300 Atchison Street on January 7, 2017 and notices were distributed to residents and business within 300-feet of the Project site in accordance with City standard practices on January 4, 2017. At the time of writing this Staff Report (January 11), the City has not received any written public comment. Written comments received by the City shall be supplied to the Commission on the day of the meeting.

**VII. RECOMMENDATION:**

Find that the project is categorically exempt under Class 32, State CEQA Guidelines 15332, and approve the Architecture and Site Plan Review and Conditional Use Permit based upon the findings contained in the attached Resolution.

**IX. ATTACHMENTS:**

1. Planning Commission Resolutions No. 2017-002 & 2017-003  
Exhibit A: Proposed Site and Floor Plans
2. Color Elevations

Respectfully Submitted By:

*Donna M. Kenney*  
Donna M. Kenney  
Planning and Building Manager



## City of Riverbank Development Services Department

Planning Division ≈ Building Division ≈ Neighborhood Improvement Division

6707 Third Street, Riverbank, CA 95367 Office (209) 863-7128 FAX (209) 869-7126

**PUBLISH DATE:** January 4, 2017  
**DEPT:** PLANNING

**LEGAL**

### **CITY OF RIVERBANK NOTICE OF PUBLIC HEARING**

Notice is hereby given that the City of Riverbank Planning Commission will conduct a public hearing to consider the projects, described below at 6:00 p.m. on Tuesday January 17, 2017, in Council Chambers 6707 Third Street, Riverbank, California.

#### **Project Description:**

**Item 3.2: Architecture and Site Plan Review 03-2016 and Conditional Use Permit 02-2016 – Jason Huckabee** – Request for architecture and site plan review of the Del Rio Theater building located at 3300 Atchison within the Downtown Specific Plan area. The proposed project includes a remodeled restaurant/piano bar, coffee shop with drive-thru, and banquet hall, a Conditional Use Permit for serving alcohol and a future lot merge. APN: 132-009-001. CEQA Categorical Exemption: Article 19, Section 15332 In-Fill Development Projects.

**The City of Riverbank will hold a Public Hearing as follows:**

**Planning Commission Meeting  
January 17, 2017 at 6:00 pm  
City Hall Council Chambers - 6707 Third Street - Riverbank, California**

**ALL INTERESTED PARTIES** are invited to attend the public hearing on **January 17, 2017** at the time and place specified above to express opinions or submit evidence for or against the subject matter being considered. Written comments via e-mail to [dkenney@riverbank.org](mailto:dkenney@riverbank.org) by postal service, or hand delivered to **6707 Third Street, Suite A, Riverbank, California, 95367**, will be accepted by the Development Services Department up to 5:00 p.m. on said date. All written comments received by said time will be distributed to the Planning Commission for consideration. Oral comments will be received by the Planning Commission prior to the close of the Public Hearing on the subject matter being considered. The Planning Commission will receive all testimony prior to taking action. Testimony cannot be given over the telephone. If you challenge the City's action on these matters in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice or in written correspondence delivered to the City at, or prior to, the public hearing.

Meeting facilities are accessible to persons with disabilities. Any person requiring special assistance to participate in the meeting should notify the Administration Dept. at (209) 863-7122 or [cityclerk@riverbank.org](mailto:cityclerk@riverbank.org) at least seventy-two (72) hours prior to the meeting. For questions regarding the public hearing matter contact Donna M. Kenney, Planning & Building Manager, at (209) 863-7124; [dkenney@riverbank.org](mailto:dkenney@riverbank.org).

Any public record materials pertaining to the presentation of the subject matter being considered will be made available for review at the Development Services Counter at 6717 Third Street, Riverbank, and (if technologically possible) at <http://www.riverbank.org/Depts/planning/default.aspx> upon distribution to a majority of the Planning Commission (typically 72 hours prior to the meeting).

**City of Riverbank  
Planning Commission  
Resolution No. 2017-002**

**ADOPTION OF ARCHITECTURE SITE PLAN REVIEW 16-0016  
DEL RIO BANQUET HALL  
3300 Atchison Street (132-009-001)**

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**WHEREAS**, An application has been received from Tony Zaia with a proposal for a Tenant Improvement for a Banquet Hall, Piano Bar and Grille, and a Coffee House with Drive-Thru including upgrades to the façade and the creation of an outdoor eating area and addition on the south side of the building, located at 3300 Atchison Street (APN 132-009-001); and

**WHEREAS**, The Planning Commission held a public hearing on January 17, 2017, to consider Architecture and Site Plan Review 16-0016; and

**WHEREAS**, The Planning Commission reviewed and considered a Notice of Exemption regarding the potential environmental effects of the proposed application, including any comments received; and

**WHEREAS**, The Riverbank Planning Commission made the following findings:

1. The proposed project, together with the provisions for its design and improvements, is consistent with the goals, policies, program and uses of the General Plan.
2. The proposed Architecture and Site Plan Review along with the Conditions of Approval is in conformity with both the intent and provisions of the Zoning Ordinance, Title 153, of the City of Riverbank Code of Ordinances.
3. Evidence was introduced in support of the application sufficient to enable the Planning Commission to find that the establishment, maintenance, or operation of the use or building applied for, under the circumstances of the particular case, will not be detrimental to the health, safety and general welfare of the persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city.

**WHEREAS**, the proposed project is consistent with the following aspects of the General Plan:

1. *Policy DESIGN-10.1*: The City will require site and building access for pedestrians and bicyclists by providing the most direct pedestrian access from sidewalks and

parking areas to building entrances while minimizing conflicts with motor vehicle traffic.

2. *Policy DESIGN-10.8*: The City will require ground floor building façade treatments and activities that generate pedestrian interest and comfort. Large windows, canopies, arcades, plazas and outdoor seating are examples of such amenities.
3. *Policy DESIGN-10.11*: The City will require incorporation of architectural and landscape features that allow for secure locking of bicycles in locations easily observed from indoors.

**WHEREAS**, the property has a General Plan Land Use Element Designation of Mixed Use; and

**WHEREAS**, the Mixed Use designation explicitly allows for development of retail uses along major roadways on the periphery of existing neighborhoods; and

**WHEREAS**, the property has a zoning designation of Main Street Retail within the Downtown Core (DC) area of the Downtown Specific Plan; and

**WHEREAS**, permitted uses in the DC District shall be subject to all provisions of the Community Commercial (C-2) District of the City's zoning code; and

**WHEREAS**, the request and plans of Yoshino-Shaw & Associates and Tony Zaia, applicant are hereby granted and approved, subject to the following conditions:

1. The applicant shall comply with the City of Riverbank Standard Conditions as contained in the Planning Commission Resolution 2013-014 and/or receive confirmation from the Planning and Building Manager that a specific condition or conditions does not apply to the subject project; and
2. This approval is dependent upon and limited to the proposals and plans contained, supporting documents submitted, presentations made to staff, Planning Commission affirmed to by the applicant. Any variation from these plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.
3. The applicant shall secure and comply with all applicable federal, state and local licenses, permits, authorizations, conditions, agreements, and orders prior to or during construction and operation, as appropriate.
4. The applicant shall comply with all regulations and code requirements of the Planning and Building Manager, City Engineer, Building Official, Stanislaus Consolidated Fire Protection District, the Police Chief and any other agencies

requiring review of the project. If required, these agencies shall be supplied copies of the final site plans, public improvement plans and building plans.

5. Resolution of approval and all conditions of approval for this project shall be transcribed by the project developer onto all building permit plan check sets submitted for review and approval. These conditions of approval shall be on, at all times, all grading and construction plans kept on the project site. It is the responsibility of the building developer to ensure that the project contractor is aware of, and abides by, all conditions of approval. Prior approval from the Planning and Building Manager must be received before any changes are constituted in site design, grading, building design, building colors or materials, etc.
6. Should the project be found, at any time, not to be in compliance with any of the Conditions of Approval, or should the applicant construct or operate this development in any way other than specified in the Application or Supporting Documents or presentations to staff or Planning Commission, as modified by the Conditions of this Approval, then the terms of this Approval shall be considered to be violated.
7. Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.
8. The developer shall pay all applicable processing fees, permit fees, fire fees, school fees, and other public entity fees in effect at the time of the issuance of a building permit.
9. The applicant shall defend, indemnify and hold harmless the City of Riverbank, its agents, officers, and employees from any claim, action, or proceeding against the City or its agents, officers, or employees to attack, set aside, void or annul this approval, or any aspect of the City's consideration of applicant's project. The applicant recognizes and agrees that applicant's voluntary commitment to meet the obligations described in this condition is an integral factor in the City's approval of this project. The intent of this condition is to require the applicant to bear the cost of any and all litigation instituted to overturn or in any way modify the City's approval of this project. Such costs include without limitation, any award of attorney's fees and costs to a prevailing plaintiff or petitioner. In the event the City becomes aware of any such claim, action, or proceeding, the City shall promptly notify the applicant and shall cooperate fully in the defense. If the City fails to promptly notify the applicant, or if the City fails to cooperate fully in the defense, the applicant shall not thereafter be responsible to defend, indemnify, or hold harmless the City. Nothing contained in this condition prohibits the City from participating in the defense of any claim, action, or proceeding, if both the following occur: (a) the City bears its own attorneys' fees and costs; and (b) the City defends the action in good faith. The applicant shall not be required to pay or perform any settlement unless the applicant approves the settlement.

10. All trash enclosures shall be constructed of masonry material with self-enclosing doors and have a second access and a sloped roof. The enclosure shall have materials and colors consistent with the primary building and be screened by landscaping.
11. All mechanical, irrigation, ground and/or roof mounted equipment shall be architecturally screened from view from all public right-of-ways prior to issuance of certificate of occupancy.
12. The Planning and Building Manager shall approve the decorative bicycle rack and plant/tree palette before installation.
13. The applicant shall submit separate application and plans for landscape and irrigation, water allowance calculations. Said plans shall comply with State water conservation standards as well as City of Riverbank standards.
14. All proposed signage shall be submitted under separate building permit applications to the City of Riverbank Building Department.
15. All utility work including electrical, gas, telephone, and cable TV shall be subject to an Encroachment Permit. Normally, the utility companies obtain separate permits for their work. It is the responsibility of the applicant to ensure that this has been done prior to beginning work.
16. All underground facilities shall be located by the appropriate agencies prior to any excavation. This shall be initiated through Underground Service Alert (U.S.A.) by calling (800) 227-2600.
17. The Contractor shall adhere to normal working hours of 6am through 6:30 pm Monday through Friday, excluding City holidays. Construction outside of normal working hours, or on Saturdays, Sundays or City holidays may be allowed upon prior approval by the City Engineer. However, weekend and City holiday work shall be limited to 8am to 5pm unless prior approval in writing is obtained from the City Engineer. Night work shall comply with all Cal Trans Standards for such work.
18. The Contractor is responsible for temporary erosion control at all times. Temporary erosion control shall consist of, but not be limited to, constructing such facilities and taking such measures as are necessary to prevent, control and abate water, mud and erosion damage to public and private property as a result of the project. Mud and silt shall be settled out of the storm runoff before said runoff leaves the construction site or enters the City storm drain systems.
19. All exterior light fixtures shall be shown on plans subject to staff review and approval. All lights attached to buildings shall provide a soft "wash" of light

against the wall. All building and parking or yard lights shall be directed and shielded away from neighboring properties and shall compliment the site and building architecture.

20. All landscape areas shall be maintained in a healthy, thriving and weed free condition. Such maintenance shall include, where appropriate, pruning, mowing, weeding, cleaning of debris and trash, fertilizing and regular watering. Whenever necessary, planting shall be replaced with other plant materials to insure continued compliance with applicable landscaping requirements. Required irrigation systems shall be fully maintained in sound operating condition with heads periodically cleaned and replaced when missing to insure continued regular watering of landscape areas, and health and vitality of landscape materials.
21. Work shall include replacement of all broken public sidewalk, curb and gutter. This work should take place when there is no further need for encroachment of heavy equipment over walks and prior to City approval of the building for occupancy. The City Construction Inspector will mark all broken curb, gutter and sidewalk for replacement upon request. Concrete patch is not acceptable.
22. Final inspection for occupancy permits will not be granted until all construction and landscaping is complete in accordance with this resolution and approved construction plans.

**NOW THEREFORE, BE IT RESOLVED** by the City of Riverbank Planning Commission that Architecture and Site Plan Review 16-0016 is hereby approved, subject to those conditions established by Resolution No. 2017-002 and as illustrated in Exhibit "A": Site Plans, Elevations.

**PASSED AND ADOPTED** by the Planning Commission of the City of Riverbank at a regular meeting held on the 17<sup>th</sup> of January, by the following vote \_\_\_\_:

**AYES:** Commissioners:

**NOES:**

**ABSENT:**

**ABSTAIN:**

**Attest:**

**Approved:**

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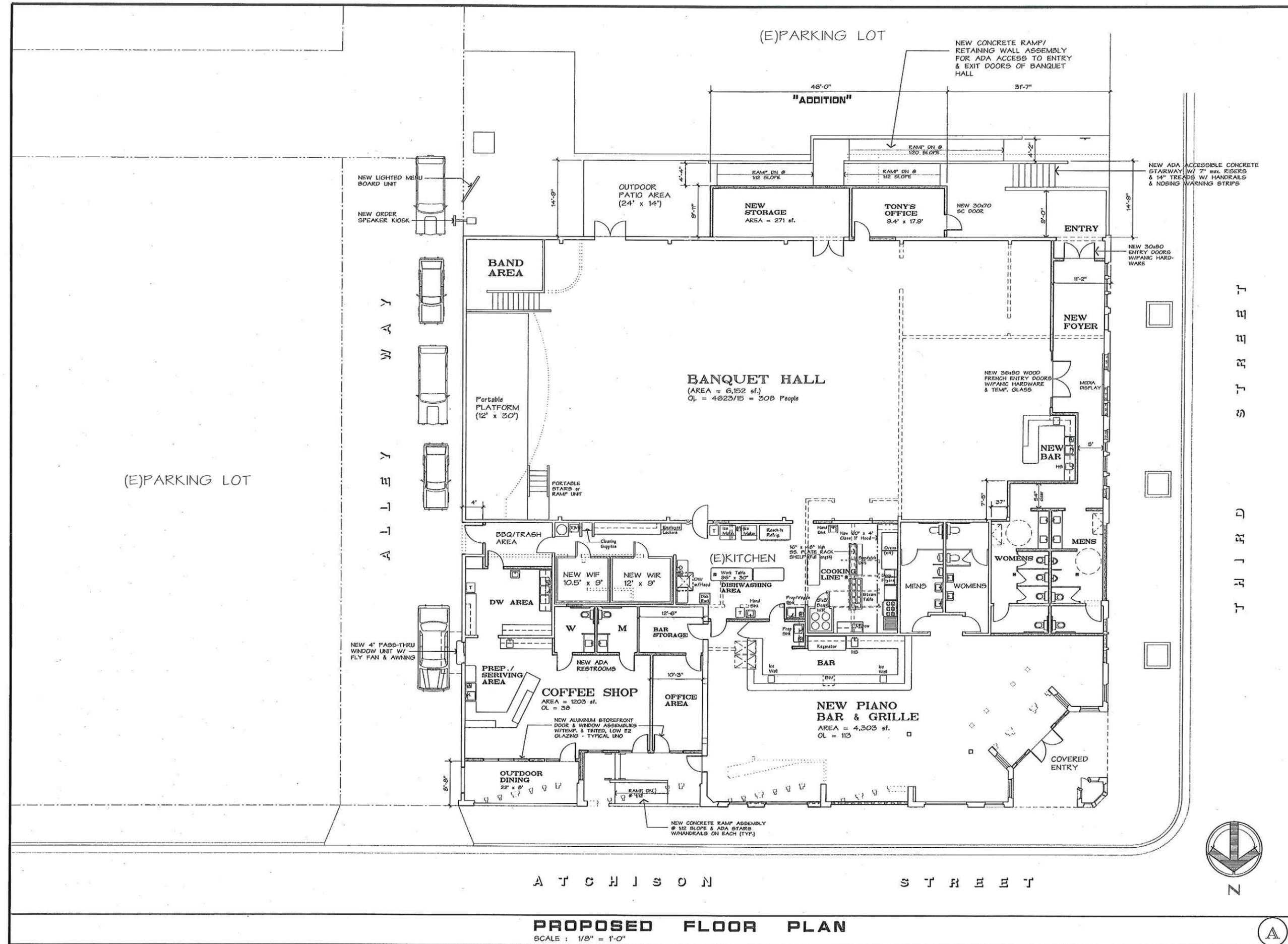
Donna M. Kenney,  
Planning and Building Manager

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Anthony McKinney, Chairperson  
Planning Commission

## Exhibit A





**PROPOSED FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

REVISIONS
1/4/17



OWNER:  
Tony Zala

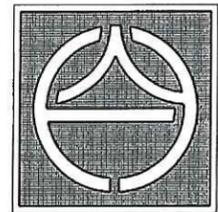
PROJECT NAME:  
**DEL RIO BANQUET HALL REMODEL**  
RIVERBANK, CA

DATE: 1/4/17  
JOB NUMBER: 1616

DRAWN BY: JDS  
JOB: JDS

DATE: 1/4/17

SHEET TITLE:  
PROPOSED FLOOR PLAN



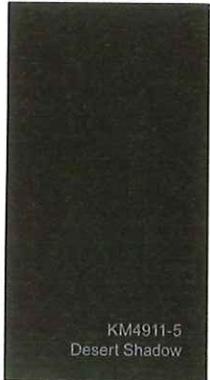
**YOSHINO-SHAW & ASSOCIATES**  
- Jim Shaw -  
**ARCHITECT**

1016 N. GOLDEN STATE BLVD  
TURLOCK, CA 95380

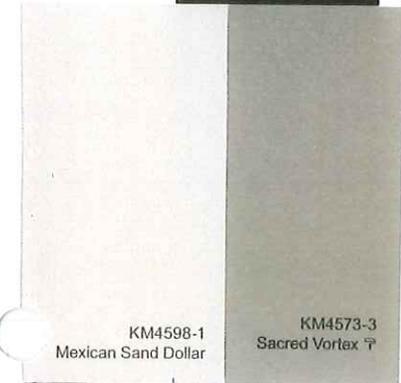
PHONE (209) 667-2603  
FAX # (209) 667-2604  
Email: jshaw@tires2wire.com

SHEET NO.  
**A-2.1**

TRIM & ACCENT COLOR #2



KM4911-5  
Desert Shadow

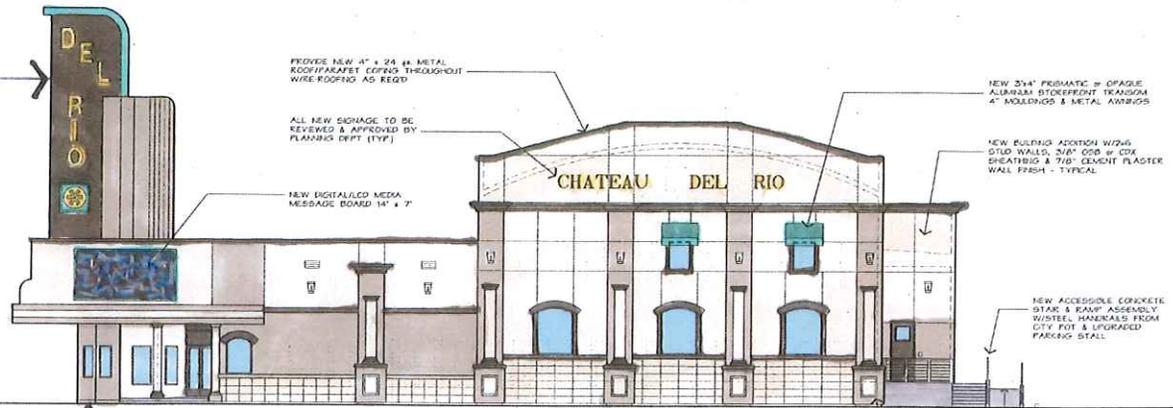


KM4598-1  
Mexican Sand Dollar

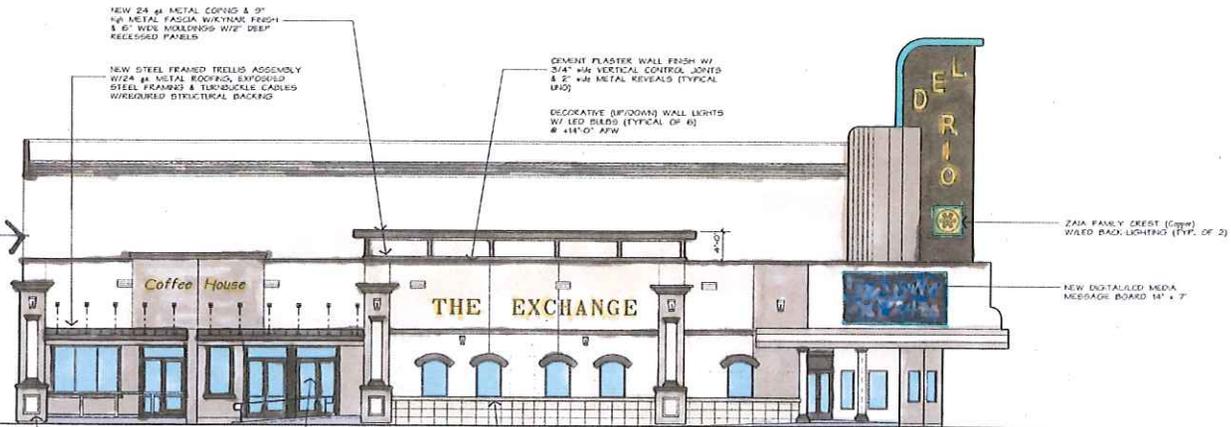
KM4573-3  
Sacred Vortex

Base Wall  
Color

Accent Color #1



WEST ELEVATION



NORTH ELEVATION

NEW EXTERIOR ELEVATIONS

SCALE: 1/8" = 1'-0"

S

A

REVISIONS



DESIGNER:  
Tony Zahr

SIGNATURE  
© 2010 Yoshino-Shaw & Associates

PROJECT NAME:  
PRELIMINARY DESIGN DRAWINGS FOR:  
DEL RIO BANQUET HALL REMODEL  
3300 ATCHISON STREET  
RIVERBANK, CA

SHEET TITLE:  
NEW EXTERIOR ELEVATIONS

DATE  
11/15/10

DRWN BY:  
JDS

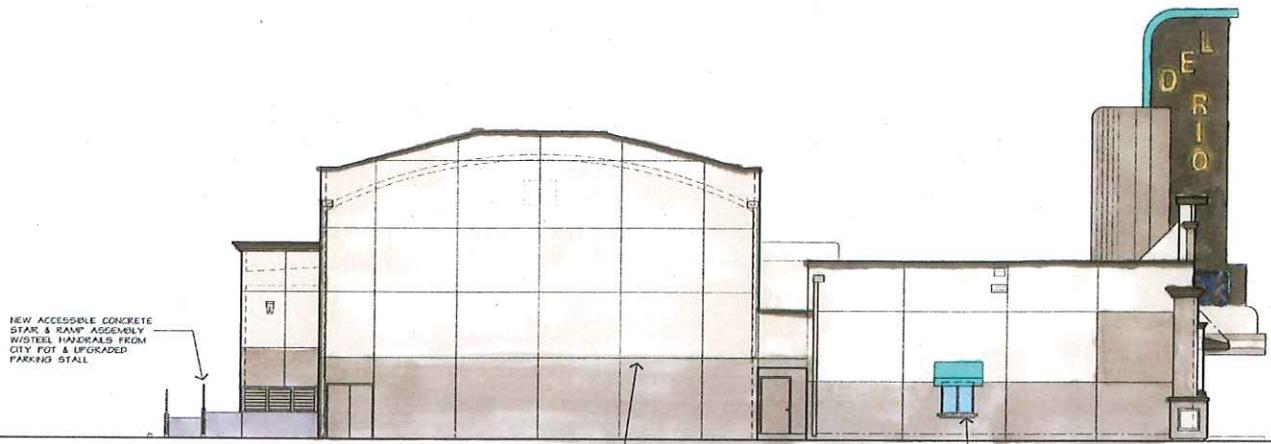
JOB NUMBER:  
1916



**YOSHINO-SHAW & ASSOCIATES**  
— Jim Shaw —  
**ARCHITECT**  
105 N. GOLDEN STATE BLVD  
TURLOCK, CA 95390

PHONE (209) 667-2605  
FAX # (209) 667-2604  
Email: jshaw@yrs2w.com

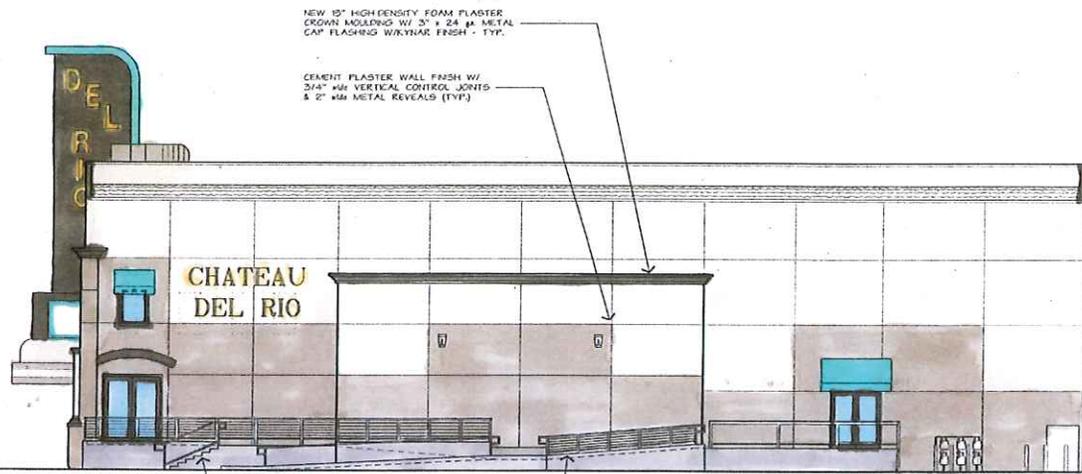
SHEET NO.  
**A-3.2**



"OPTION" : TO INSTALL NEW  
3/4" x 1/2" VERTICAL CONTROL  
JOINTS & 2" x 1/2" HORIZONTAL  
REVEALS AT (EXISTING) WALLS

NEW 4' PASS-THRU WINDOW  
UNIT W/ WAFFLE PANEL AWNING  
& STAINLESS STEEL COUNTER

EAST ELEVATION



NEW 10" HIGH DENSITY FOAM PLASTER  
CROWN MOLDING W/ 3" x 24 # METAL  
CAP FLASHING W/ WYVAK FINISH - TYP.

CEMENT PLASTER WALL FINISH W/  
3/4" x 1/2" VERTICAL CONTROL JOINTS  
& 2" x 1/2" METAL REVEALS (TYP)

NEW ACCESSIBLE CONCRETE STAIR  
& RAMP'S W/ WATTLE HANDRAILS  
FROM CITY POT & UPGRADED  
PARKING STALL

SOUTH ELEVATION

**NEW EXTERIOR ELEVATIONS**  
SCALE : 1/8" = 1'-0"

REVISIONS



OWNER :  
Tony Zala

**DEL RIO BANQUET HALL REMODEL**  
RIVERDANK, CA

STATION

© 2015 YOSHINO-SHAW & ASSOCIATES

PRELIMINARY DESIGN DRAWINGS FOR :

**DEL RIO BANQUET HALL REMODEL**  
3300 ATCHISON STREET

JOB NUMBER:  
1516

DRAWN BY:  
JDS

DATE  
11/15/16

SHEET TITLE:  
NEW EXTERIOR ELEVATIONS  
CONT.



**YOSHINO-SHAW & ASSOCIATES**  
- Jim Shaw -  
ARCHITECT

1016 N. GOLDEN STATE BLVD  
TURLOCK, CA 95380

PHONE (209) 667-2603  
FAX # (209) 667-2604  
Email : jshaw@freedom.com

SHEET NO.  
**A-3.3**

(S)

(A)

**CITY OF RIVERBANK  
PLANNING COMMISSION  
STAFF REPORT**

**ITEM NO:** 3.3 January 17, 2017

**APPLICATION:** Continuation of Revocation of Conditional Use Permit No. 05-2007 and Business License at 6607 Callander Avenue, 209 Concepts – Action on Code Enforcement Case #2015-0209.

**PROPERTY OWNERS:** Arnulfo and Maria Sanguino

**BUSINESS OWNER  
209 CONCEPTS:** Nicholas Lopez

**LOCATION:** 6607 Callander Avenue, rear ½ of building  
APN: 075-004-046.

**GENERAL PLAN  
DESIGNATION:** Community Commercial (C/C)

**DOWNTOWN  
SPECIFIC PLAN:** Highway Boulevard

**PROJECT PLANNER:** Donna M. Kenney, Planning and Building Manager

**RECOMMENDATION:** Revoke Conditional Use Permit (CUP) No. 05-2007 and the Business License for 209 Concepts based on the Municipal Code and CUP violations.

**ACRONYMS:** CUP – Conditional Use Permit  
RMC – Riverbank Municipal Code

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**I. EXECUTIVE SUMMARY:**

209 Concepts, an automobile repair and paint business at 6607 Callander has been found in violation of zoning code requirements and conditions of approval attached to the property's conditional use permit for the use of an auto paint

booth. Documented violations go back to 2010 and, although city staff has sent letters, conducted on-site visits, and met with the business owner at City Hall on several occasions, the same violations keep occurring. At their regular meeting of December 20, 2016 the Planning Commission voted to continue this item for 30 days so that the business owner has an opportunity to address the violations.

The Violations include the items below. Staff had a site visit on January 11, 2017 and found:

1. Pursuant to RMC 153.092 (Q)(2), all work shall be conducted within a building. Painted vehicle parts have been observed drying outside of the building. The outdoor sink has been removed.
2. Pursuant to RMC 153.092 (Q)(3), the businesses must meet off street parking requirements. The rear parking area still contains one temporary structure. While the business owner has painted lines for two parking spaces, staff did not see a dedicated handicap parking space.
3. Pursuant to RMC 153.092 (Q)(4), cars to be stored more than 24 hours shall be stored within the building or storage area surrounded by a six (6) foot high solid fence (out of the required setback areas). The new business in front has had cars parked outdoors overnight because 209 Concepts business owner Nicholas Lopez still has not removed a vehicle lift and disabled vehicles from within the front bays or business items from the front office.
4. Pursuant to RMC 153.094 (A)(2), no fence that limits visibility shall be constructed in excess of three (3) feet in height within the front or side yard of a corner lot. Most tarps, wood and metal have been removed from the permitted six (6) foot wrought iron fence but one piece still remains attached.
5. Pursuant to Section 105.1 of the 2013 CA Building Code, submit plans and applications for any structures built without a permit. There is still one temporary accessory structure located in the rear parking area.
6. According to a Condition of Approval placed upon the project by the Planning Commission on April 17, 2007, the business owner shall keep the building in good repair and general appearance and not allow any business activity on the site to interfere with the peaceful enjoyment of neighboring sites. Wrecked vehicles and junk are more organized but still kept outside the building. There is still a junk desk in full view of the street.

In reviewing any use permit, the applicant must introduce evidence in support of his case sufficient to enable the Planning Commission to find that the establishment, maintenance, or operation of the use or building, under the circumstances of the particular case, would not be detrimental to the health, safety and general welfare of the persons residing or working in the neighborhood of such proposed use or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city.

**V. RECOMMENDATION:**

Recommendation is to revoke Conditional Use Permit 05-2007 and the business license for 209 Concepts based on a history of continued violations to Municipal Code sections and required Conditions of Approval.

Respectfully Submitted By:

*Donna M. Kenney*  
Donna M. Kenney  
Planning and Building Manager

**Attachments:**

1. Resolution No. 2017-004
2. Photos

**City of Riverbank  
Planning Commission  
Resolution No. 2017-004**

Resolution of the City of Riverbank Planning Commission  
Revoking Conditional Use Permit No. 05-2007 for an auto paint booth  
and the Business License for 209 Concepts at  
6607 Callander Avenue (APN: 075-004-046)

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**Whereas,** The Planning Commission held a public hearing on December 20, 2016 to consider revoking Conditional Use Permit (CUP) No. 05-2007 for an auto paint booth and the business license for 209 Concepts located at 6607 Callander Avenue, zoned as Highway Boulevard in the Downtown Specific Plan, for repeated violations of the CUP's Conditions of Approval; and

**Whereas,** The Planning Commission reviewed and considered the appropriate documents regarding the effects of the violations; and

**Whereas,** The Planning Commission of the City of Riverbank could not make the following findings to keep the CUP in effect:

1. The site is physically suitable for the proposed type and intensity of development permitted by CUP No. 05-2007 and does not cause any impacts on surrounding properties.

*Pursuant to RMC 153.092 (Q)(2), all work shall be conducted within a building. The business owner of 209 Concepts stated to staff he does not have enough room indoors for all the vehicles he wants to work on. Vehicle parts have been observed by city staff outside of the building drying.*

*Pursuant to Section 105.1 of the 2013 CA Building Code, submit plans and applications for any structures built without a permit. One temporary accessory structure remains.*

*Pursuant to RMC 153.094 (A)(2), no fence that limits visibility shall be constructed in excess of three (3) feet in height within the front or side yard of a corner lot. Most of the attached tarps, metal, and plywood screening which has obstructed the view of vehicles entering and leaving the rear side parking lot have been removed. One remains.*

2. The approval of CUP No. 05-2007 for the property located at 6607 Callander Avenue is not detrimental to the health, safety, peace, morals, comfort and general welfare of persons residing or working in the neighborhood.

*Pursuant to RMC 153.092 (Q)(3), businesses must meet off street parking requirements. The rear parking area contains one temporary structure blocking required parking spaces. Staff did not see a handicap parking space.*

*Pursuant to RMC 153.092 (Q)(4), cars to be stored more than 24 hours shall be stored within the building or storage area surrounded by a six (6) foot high solid fence (out of the required setback areas). 209 Concepts business owner Nicholas Lopez still has a lift and disabled vehicle in the front bays, keeping that front business owner from moving his vehicles within the building.*

3. According to a Condition of Approval placed upon the project by the Planning Commission on April 17, 2007, the business owner shall keep the building in good repair and general appearance and not allow any business activity on the site to interfere with the peaceful enjoyment of neighboring sites.

*There are aesthetic, noise, air quality, and parking impacts created by this business that interfere with the peaceful enjoyment of neighboring sites, including the outdoor storage of junk and vehicles, and temporary structures causing impacts to street parking.*

**Therefore, Be It Resolved**, the Planning Commission of the City of Riverbank hereby finds based on sufficient evidence supplied by the applicant and evidence demonstrated in the public record that the establishment, maintenance and operation of the use and business has, under the circumstances of this particular case, been detrimental to the health, safety, and general welfare of persons residing and working in the neighborhood of such use and business and is detrimental and injurious to property and improvements in the neighborhood and to the general welfare of the city. The Planning Commission hereby revokes the use permit to allow the auto paint booth and the business license for 209 Concepts, located at 6607 Callander Avenue.

**Passed and adopted** by the Planning Commission of the City of Riverbank at a regular meeting held on the 17<sup>th</sup> day of January 2017, by the following vote:

**AYES:** Commissioner's:

**NAYS:**

**ABSENT:**

**ABSTAIN:**

**Attest:**

**Approved:**

Riverbank Planning Commission  
January 17, 2017  
Resolution No. 2017-004

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**Donna M. Kenney, Secretary  
Planning and Building Manager**

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**Anthony McKinney, Chairperson**

**BODY SHOP**











SPEED  
LIMIT  
25

NO  
LEFT TURN  
ON RED

CHEVROLET  
SILVERADO

